

**MINUTES  
PLANNING COMMISSION  
CITY OF LAKEWOOD  
12650 DETROIT AVENUE  
AUDITORIUM  
NOVEMBER 7, 2024  
6:00 P.M.  
RECORDED**

(The meeting was called to order at 30:28 on the recording)

**1. ROLL CALL**

Members Present

Hannah Gall  
Nicholas LaPointe, Vice Chair  
Sean McDermott, Chair  
William Sanderson

Staff Present

David Baas, Assistant Director, Planning and Development  
Jen Swallow, Chief Assistant Law Director  
Mark Papke, Engineer

A motion was made by Mr. McDermott, seconded by Mr. LaPointe to **EXCUSE** the absence of Kyle Reisz. All of the members voted yea; the motion passed.

**2. APPROVE THE MINUTES OF THE OCTOBER 3, 2024 MEETING**

A motion was made by Mr. LaPointe, seconded by Ms. Gall to **APPROVE** the October 3, 2024 meeting minutes. All of the members voted yea; the motion passed.

**3. OPENING REMARKS**

Staff read the Opening Remarks into record.

**NEW BUSINESS**

**COMMUNICATION**

**4. Docket 11-42-24**

**Communication from Planning and Development Staff**

**Lake-Clifton Connector Project (Cuyahoga County): Resolution No. 2024-68**

At the October 21, 2024 meeting City Council was presented with an agreement between the City and Cuyahoga County for the subject project that proposes improvements for Clifton Boulevard from Linda Street to Webb Road within the cities of Lakewood and Rocky River including repurposing the eastbound lanes (into one eastbound lane and one westbound lane), repurposing the westbound lanes (into a linear park and shared use path), installation of a roundabout at Lake Road, installation of a mid-block crossing at Forest Road, new decorative street lighting, tree plantings, and other appurtenances. City Council referred the project to Planning Commission for review. (Page 88)

Trevor Hunt, Senior Project Manager, Lakefront Projects, Cuyahoga County Department of Public Works, and Matt Hils, Principal - OHM Advisors, were present to explain the project. Staff provided brief introduction and connection of the project to active transportation studies and plans. Public comment was closed as no one addressed the item.

## Commissioners Comments/Questions

- Will there be changes to the bridge?
  - At this time, the County is studying for new fencing, lighting, and painting of the bridge and pedestrian lane striping. Any future improvements will not be covered by the Lakewood share, and will be done by the County,
- What about a parking plan for visitors after bridge deck improvements are made?
  - The plan for bridge deck improvements was removed because the City of Rocky River deemed it to be a detriment.
- The agreement reads that truck traffic is permitted, but Lakewood does not allow any truck weighing two or greater to traverse from West Clifton Blvd. along Clifton Blvd.
  - Ms. Swallow said that Lakewood's restriction was approved by the Ohio Revised Code.
- The agreement reads there cannot be a prohibition of no parking.
  - The on-street parking spaces between West Clifton Blvd. and Webb Rd. would remain as is currently. Both ODOT and RTA were in agreement with one lane being removed and the on-street parking.
- The existing fences are disparate. Could the project include uniform fencing?
  - The seventeen fences are within ODOT's limited access purview, could remove them, and is working with Lakewood's Public Works Department and P&D.
- Possibility of establishing fence standards.
- Would the WTEs cause traffic back-up issues?
  - A pull-off lane is included for one stop, traffic would have to stop for the others.
- Will the stop sign remain at Clifton Rd. intersection?
  - The Clifton Rd. intersection will have a traffic light.
- Who will maintain the Lakewood portion?
  - The City of Lakewood will.
- Will the shared use path be plowed in the winter?
  - It will be plowed by the City of Lakewood.
- The bike lanes on Lake Ave. end at Webb Rd. The shared use path is supposed to go all the way to Webb Rd. but is not shown that way.
  - Currently there is a disconnect between the Web Rd./Clifton Rd. intersection and the Webb Rd./Lake Ave. intersection – not enough room to fit a trail in ROW on Webb Rd.
- The connection from West Clifton Blvd. to Webb Rd. should be done.

A motion was made by Mr. McDermott, seconded by Mr. LaPointe, to **APPROVE the Communication and refer to City Council with the following conditions:**

- **The project includes new fencing on both sides of the right-of-way from West Clifton Boulevard to the bridge approach.**
- **If the project cannot include the fencing, a plan is put in place for uniformity, over time, when the fences are replaced in the limited access right-of-way.**

All of the member voted yea; the motion passed.

## OLD BUSINESS

## CONDITIONAL USE

5. **Docket 10-38-24**  
**16306 Detroit Ave.**  
**Jersey Mike's Subs**

Steven Minnich, NE OHIO Subs LLC, applicant requests the review and approval for a conditional use permit to operate seasonal outdoor dining, pursuant to section 1129.13 – supplemental regulations for outdoor/ seasonal dining facility. The property is in the C3, Commercial General Business district. (Page 82)

Heather Tuholski, Director of Fields Operation, representative and Victor Ruiz, were present to explain the request. Mr. Baas confirmed the dimensions of the walkway for the project (from rear parking lot to Detroit Ave.) and provided administrative comments.

#### Commissioners Comments/Questions

- The lack of visual control was a concern, but it is private property.
- Asked that the area was included in the lease.
- Confirmed the exact location of the fence.
- Asked about ADA and service.
- Did not like the fence around the patio.
- Landlord required the fence.

Public comment was closed as no one addressed the item. No comments were received before the meeting.

A motion was made by Mr. Sanderson, seconded by Ms. Gall to **APPROVE** the request as submitted. Mr. Sanderson, Ms. Gall, and Mr. LaPointe voted yea, and Mr. McDermott voted nay; the motion passed.

#### OLD BUSINESS

#### COMMUNICATION

##### **6. Docket 07-30-24**

##### **Communication from Planning and Development Staff Complete Streets Initiative: Ordinance No. 11-2024**

At the June 17, 2024 City Council meeting, this item was referred to the Planning Commission for its review.

- Ordinance No. 11-2024 to enact Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood, to address the livability and safety needs of Lakewood by building upon efforts to promote an equitable multimodal transportation system.

(Page 3)

##### **7. Docket 07-31-24**

##### **Communication from Planning and Development Staff Complete Streets Initiative: Resolution No. 2024-42**

At the June 17, 2024 City Council meeting, this item was referred to the Planning Commission for its review.

- Resolution No. 2024-42 to support the Complete Streets Initiative and to address the livability and safety needs of Lakewood by building upon efforts to promote an equitable multimodal transportation system.

(Page 20)

Docket 07-30-24 and Docket 07-31-24 were taken together. Mr. Baas stated there was a consolidated draft that was not ready for review and recommended deferral.

A motion was made by Mr. McDermott, seconded by Mr. Sanderson to **DEFER Docket 07-30-24 and Docket 07-31-24**. All of the members voted yea; the motion passed.

## **PLANNED DEVELOPMENT**

### **8. Docket 10-37-24**

#### **Irene & Donald Ave.**

#### **Donald Thoreau Development**

Susan Broadwater, Beegan Architectural Design, applicant requests the review and final approval of a planned development consisting of ten townhouse units and one single-family house, pursuant to section 1156 – planned development. The property is in an R2, Single and Two-Family district. (Page 60)

Mr. Baas presented a brief overview of the proposal. Paul Beegan, Beegan Architectural Design, was present to explain the updates. Mr. Baas provided administrative comments synopsis of previous meetings. Public Comment was taken.

#### Public Comments/Questions

- Concerned that the buffering, mature Juniper trees would not screen the balconies/lights adequately.
- Wants to check property line.
- The design does not match the character of the neighborhood.
- It is unsafe not to have fencing between the railroad tracks and the structures.

#### Commissioners Comments/Questions

- Will the project get ABR review, and specifically the landscaping?
  - Mr. Baas replied to the affirmative. For this new development, landscaping and streetscape are under PC and ABR reviews.
- Is the sidewalk access path open to the public?
  - Yes.
- Understand everyone's concerns, but this is in compliance with R2 and ADUs.
- Is there any other vegetation that would survive, thrive, and provide better screening?
  - Yes, would be open to other options.
- What could be done to prevent cut-through traffic with removal of the bollards?
  - Proper signage would be used.
- Suggested that ABR provides direction for a living wall.
- Sublot 12 will be owned by whom?
  - An association although it is not defined fully yet.
- Is Sublot 12 for visitor parking?
  - Yes.
- Asked about the fibre optic line, perhaps need an easement with the fibre optic line owner.
  - Having discussion about it already.

A motion was made by Mr. Sanderson, seconded by Ms. Gall to **APPROVE the request with the following conditions:**

- **The plat is reviewed and approved by the City Engineer.**
- **The landscaping, curves, and buffer between the drive and fence area are discussed as part of ABR review.**

All of the members voted yea; the motion passed.

## **NEW BUSINESS**

### **PARKING PLAN**

#### **9. Docket 11-39-24 1517 Alameda Ave.**

Marco Oriella, property owner and applicant, requests review and approval for the exception to the parking minimum for a residential lot, pursuant to sections 1143.10 – exceptions to required minimums and 1143.09 – parking plan review: Planning Commission. The property is in an R2, Single and Two-Family district. (Page 55)

Marco Oriella, property owner and applicant was present to explain the request. Mr. Baas provided administrative comments and recommended approval. Mr. Papke said it was a good solution to reduce the amount of impervious surface and storm runoff. Mr. Baas stated that in terms of exceptions to the residential garage requirement, this is what one would like to see for single-family homes. Public comment was closed as no one addressed the item.

#### **Commissioners Comments/Questions**

- How many vehicles do you own?
  - Two vehicles.
- Where will you park the cars?
  - In the driveway adjacent to the house.
- Is there a schedule for garage demolition and plantings?
  - Landscaping is weather dependent, hope to have the garage removed by end of the year, shed would be erected in the spring along with the planting beds, and composting would be done as soon as possible.
- Upon completion, would you allow your area to be used as an example?
  - "I would be honored.\*"
- Does this have to go to ABR?
  - It is not visible from the ROW. A demolition permit is required.
- It needs to be confirmed that the driveway is wide enough between the two houses to accommodate the opening of car doors.
  - Ms. Swallow confirmed the width is ten feet (10').
- On lieu of a shed, the plan could be accomplished with a one-car garage.
- Stated that one covered parking space is required.

Discussion continued about former parking plan approvals; each one was unique and non-conforming. Parking minimum and garage requirements will be included in the Zoning Code Refresh. Mr. Oriella said one garage door did not open, the garage slanted enough that he would not park his car within it, and he planned to make it an investment for the community. The Commission discouraged the use of a Shagbark Hickory tree. Mr. Oriella does not value a garage and driveway and is not concerned about resale value; trees added value to a property. The Commissioners were concerned about setting a precedence and

suggested to discuss the proposal with the neighbors. Mr. Baas stated that a Public Notice letter was mailed as mandated. The proposed shed was 124 square feet.

A motion was made by Mr. LaPointe, seconded by Ms. Gall to **APPROVE the request with the following conditions:**

- **The proposed work is completed within one year of the garage’s removal.**
- **The proposed is maintained within the property maintenance code.**

Ms. Gall, Mr. McDermott, Mr. LaPointe voted yea, and Mr. Sanderson abstained; the motion passed.

**COMMUNICATION**

**10. Docket No. 11-40-24**

**Communication from Planning and Development Staff**

**ODOT Excess Land Transfer: Dedication of Portion of Glenbury Avenue as Public Right-of-Way: Ordinance 18-2024**

At the October 7, 2024 meeting City Council was presented with legislation, which if adopted would dedicate a small portion of Glenbury Avenue as public right-of-way. City Council referred the legislation to the Planning Commission for review. (Page 74)

Mr. Papke stated ODOT wanted the ROW dedicated prior to transfer of the property to the City of Lakewood. Public comment was closed as no one addressed the item.

A motion was made by Mr. Sanderson, seconded by Mr. LaPointe to **APPROVE** the dedication. All of the members voted yea; the motion passed.

**COMMUNICATION**

**11. Docket No. 11-41-24**

**Communication from Planning and Development Staff**

**ODOT Excess Land Transfer:  
Vacation of Portion of Niagara Drive as Public Right-of-Way: Ordinance 17-2024**

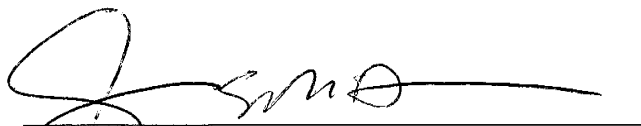
At the October 7, 2024 meeting City Council was presented with legislation, vacating a small portion of Niagara Drive from the public right-of-way. City Council referred the legislation to the Planning Commission for review. (Page 81)

Mr. Papke stated ODOT wanted the ROW vacated prior to transfer of the property to the City of Lakewood. It will be used as part of Niagara Park. Public comment was closed as no one addressed the item.

A motion was made by Mr. LaPointe, seconded by Ms. Gall to **APPROVE** of the vacation. All of the members voted yea; the motion passed.

**ADJOURN**

Mr. McDermott **ADJOURNED** the meeting at 8:35 p.m.

  
 \_\_\_\_\_  
 Signature

12/5/24  
 \_\_\_\_\_  
 Date



Oath

*(You need not give an oath if you object. If you object to giving an oath, please notify the hearing officer or secretary before signing below.)*

I, the undersigned, hereby solemnly swear that the testimony I give at this proceeding will be the truth, the whole truth and nothing but the truth:

PRINT NAME:

SIGN NAME:

1. Trevor Hunt

*[Signature]*

2. Heather T. Hulst

Director of Field Op.

3. Victor Ruiz

*[Signature]*

4. PAUL BEEGAN

*[Signature]*

5. DAVE KARMOPE

*[Signature]*

6. Ken Thomas

*[Signature]*

7. Marco Oriella

*[Signature]*

8. \_\_\_\_\_

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9. \_\_\_\_\_

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10. \_\_\_\_\_

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11. \_\_\_\_\_

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Prepared by: The City of Lakewood Law Department, 12650 Detroit Ave., Lakewood, Ohio 44107

FOR CITY USE ONLY

Lakewood Administrative Procedure:  ABR/BBS  Citizens Advisory  Civil Svc.  Dangerous Dog  Income Tax Appeals  Loan Approval  Nuisance Abatement Appeals  Parking  Planning  Zoning Appeals  Other:

Date of Proceeding: Thursday, November 7, 2024

page 1 of 1

## Johanna Schwarz

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**From:** Abby Tarnai <abbyetarnai@gmail.com>  
**Sent:** Wednesday, November 6, 2024 12:53 PM  
**To:** Planning Dept  
**Subject:** Concerns re: Thoreau development

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hi -

I just want to comment on the Thoreau Rd development. I am not against development. I would like to make sure that the sidewalk is considered public. This will allow community members from the Donald side easier access to Cove Park.

And I would like to make sure the street is open to both Donald and Thoreau. This would lessen the traffic load on Thoreau which I think is especially important since it is often used as a thoroughfare for emergency vehicles.

I do have concern about the grade of the Thoreau street approaching the railroad crossing from the south. The large "dip" before the tracks may impair visibility for the new vehicle and foot traffic access. People often use the western side of the street to avoid the "dip." I think this should be reviewed for safety and addressed if needed.

I also am concerned about water retention and drainage. During a heavy rainstorm, the area for proposed development may retain water for several days, and with the space being developed and paved, I'm wondering where the water might go and question if the sewers in the area are sufficient.

I appreciate your thoughtful review of my concerns.

Thank you, Abby Tarnai  
1267 Thoreau

Sent from my iPhone

## Johanna Schwarz

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**From:** Benjamin Kroeck <kroeckbw@gmail.com>  
**Sent:** Wednesday, November 6, 2024 8:36 PM  
**To:** Planning Dept; Amanda L. Cramer  
**Cc:** Cindy A. Marx; Tom Bullock  
**Subject:** Re: 06-58-24 - Irene/Donald Development

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Planning Commission and Amanda,

Unfortunately, I am headed on a family trip to Pittsburgh tomorrow evening and will be unable to attend the entire meeting. However, I am able to make the pre-reviewal meeting at 5:30PM if this is open to the public. Please confirm?

After seeing the updated plans, I'm still concerned that there are issues that were left unresolved from the previous Planning Commission meeting.

The landscape screening continues to be inadequate. Not entirely sure that the 6' fence follows PD zoning code...Was this screening decision made in coordination with the adjacent neighbors?

Continue to believe that the proposed design would be better for the existing neighborhood if the trail and private driveway were swapped. It's encouraging to see what I believe it states is now a public easement for the multipurpose trail. Hopeful that costs of maintaining this do not fall on the city, but would have liked to see it widened to 10 feet. I recall at the last Planning Commission Meeting that the applicant was also asked to show how the trail from Cove Park would be addressed with utility wires that will have to be moved. Will this be paid for by the Developer and has it been addressed? It's unclear from the current designs.

Re-zoning from residential to planned development in an area that is completely residential should meet a threshold of assisting the public while caring for existing neighbors. I do not believe that this reaches the threshold and the bias to allowing the rezoning should be borne entirely by the developer. I firmly believe that no public funding should be used in offsetting the multipurpose trail, the connection to Cove Park, and/or any part of this project.

I continue to be opposed to rezoning this parcel from residential to planned development and would much prefer to see single family homes on the lots.

Thank you for allowing these comments to be entered into the public record.

Kind Regards,

Ben Kroeck  
1237 Thoreau Road

Lakewood, Ohio 44107

On Mon, Aug 5, 2024 at 5:57 PM Benjamin Kroeck <[kroeckbw@gmail.com](mailto:kroeckbw@gmail.com)> wrote:

I'm writing to submit a comment for Docket No. 06-58-24 for the upcoming ABR meeting. My name is Ben Kroeck, homeowner and resident at 1237 Thoreau Rd, Lakewood, OH 44107, along with my wife and two children under the age of five. We have lived here since 2015.

Our concern with the plans as proposed are simple, we feel the building zone as it stands at R-2 is why we moved into this neighborhood and specifically this street. We see no reason for this property to be rezoned to Planned Development. We chose to live in our residential neighborhood and expect that the zoning code will be enforced.

We walk everyday to Cove Park entering across the street at the proposed exit. Traffic on this street is already a safety concern. After meeting with Councilwoman Marx to discuss this project, I understand if rezoned then the Planned Development would also have to hire their own refuse removal and snow removal leading to additional traffic since it will be a private drive. Ten units total would likely mean a minimum of an additional twenty cars, plus all of the additional traffic of deliveries, visitors, etc. Our street is already a busy thoroughfare for fire/rescue with the Fire Department behind us on Clifton and Police reporting to their station on Detroit. We do not need additional traffic as there are many young families that live here like us and walk everyday to Cove Park entering off Thoreau.

I understand that this parcel has every right to be developed, but I think it should be done within our existing code. Please do not rezone these parcels. Allow as many single family homes as can reasonably fit, but building townhomes in this residential community does not take into consideration us as existing neighbors.

Perhaps the private drive should also run along the railroad tracks to take into account the existing neighbors? Look forward to additional engagement as this process continues.

Sincerely,  
Ben Kroeck  
1237 Thoreau Road  
Lakewood, Ohio 44107

## Johanna Schwarz

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**From:** Donny Davis <donny.davis4@gmail.com>  
**Sent:** Wednesday, November 6, 2024 9:12 PM  
**To:** planning@lakewoodohio.gov; Planning Dept  
**Cc:** Cindy A. Marx; Tom Bullock  
**Subject:** Docket No 10-37-24 - Donald & Jacquelyn Davis - 11.7.24  
**Attachments:** Docket No 10-37-24 - Donald & Jacquelyn Davis - 11.7.24\_Combined.pdf

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Good evening:

Please find my letter attached for inclusion in the development process regarding the proposed project near my residence at 1280 Thoreau Road.

Thank you,  
Donny Davis  
330-807-3046

Donny & Jackie Davis  
1280 Thoreau Road  
Lakewood, OH 44107

November 6, 2024

City of Lakewood Planning Commission  
12650 Detroit Avenue  
Lakewood, OH 44107

Re: Docket No: 10-37-24

Members of the Planning Commission:

We are residents at 1280 Thoreau. Our property is contiguous with adjacent yard owned by Mr. Kermode and within viewshed of the site from various angles. Thank you for your consideration of my previous letters and in-person attendance/comments to the prior meetings on this proposed development. As a neighboring property owner, I also share similar concerns as my neighbor with a contiguous yard who has also submitted a host of other concerns. The concessions have not been substantive nor satisfactory. These recent renderings were recently provided as not part of the original submittal, which would have been insightful and informative before a conditional approval. They are only being provided as part of subsequent submittals but are quite consequential to overall outcomes. This should be cause for concern and further evaluation by both the Planning Department and City of Lakewood Planning Commission in terms of preparedness and qualifications of the proposing developer. Accordingly, and as previously indicated, this developer has not provided any prior examples of portfolio work not only in the City of Lakewood, but in the Northeast Ohio region as well. Please find prior and additional concerns regarding Hilane Realty's credentials threaded in this document as additional context. In terms of this third re-visit at Planning Commission, I would like to submit the following new comment for the record.

This is an insular part of the neighborhood and therefore, while a re-zone could be warranted to achieve this density on a major arterial, I do not believe this passes the requirements for a Planned Unit Development (PUD) at its core. Nor is it characteristic of the surrounding neighborhood while protecting values and sense-of-place. My primary question now, is whether or not the following rendering/design is appropriate for a development of such obtrusive scale. I kindly ask the Planning Commission to reconsider the unit count and heights of such dwellings. The design creates a significant wall effect, which will be exacerbated by lighting, HVAC, garages, and the continuous traffic patterns for a new cut through along a garden-style property. Again, this is in an insular part of the neighborhood near an established single and two-family community next to a city park. Please defer to prior comments that I have submitted for further concerns in conjunction with. In closing, as a master's degree from University of Akron in Urban Planning & GIS, I do support the public right-of-away along the tracks for pedestrian continuity, however I believe this can be achieved regardless of this density or development.



I welcome members of Commission to a site visit for further discussion. Thank you for your thoughtful consideration of this outcome.

Regards,  
Donny Davis

330.807.3046

Donny & Jackie Davis  
1280 Thoreau Road  
Lakewood, OH 44107

October 2, 2024

City of Lakewood Planning Commission  
12650 Detroit Avenue  
Lakewood, OH 44107

Re: Docket No: 10-37-24

Members of the Planning Commission:

We are residents at 1280 Thoreau. Our property is contiguous with adjacent yard owned by Mr. Kermode and within viewshed of the site from various angles. That said, we are writing to express our opposition regarding the proposed townhome development at Donald/Thoreau on the Norfolk Southern property. Though there are many, here are the key concerns below:

The proposed re-zoning of the R-2 parcel to PD creates significantly more density, traffic, light and noise pollution, and introduces housing product that is not characteristic of the surrounding neighborhood. Additionally, with the proposed heights, the housing towers over the surrounding and insular part of the neighborhood - versus being on a main arterial like Detroit or Clifton. The rear balconies will also infringe upon the privacy that neighbors currently enjoy. The consensus among neighbors is that detached single-family residential housing could exist under current R-2, however the sheer density of this re-zone and proposal has too many ancillary negative impacts.

Rezoning a parcel of this size should be tabled until the 2024 Lakewood Zoning Refresh is complete, as the outcomes and recommendations could have impacts on current proposal based on any new guidelines/processes.

Additionally, despite attending meetings with applicant and ARB, there have been no design or orientation concessions/alterations to proposed plan throughout the process. We understand the Planning Commission/ARB does appreciate the trail introduced for connectivity to Cove Park from Donald to Thoreau, however this is only one small section of the railroad tracks that runs through the entire city and could be achieved without the development along with longer-term mobility visioning.

Finally, we would like for City Council/Planning Commission to help residents understand more about the out-of-state developer, Hilane Realty, and their credentials/ability to build the proposed product that is of lasting quality. Observationally, the company's website ([www.hilanerealty.com](http://www.hilanerealty.com)) is generic and shows no previous development portfolio work to indicate its proven track record. Instead, the company website shows only that it owns one multi-family property in Shaker Heights.

Thank you for your consideration.

Donny & Jackie Davis  
1280 Thoreau Road  
330-807-3046

Re: Docket 06-21-24

Members of Planning Commission and Architectural Review Board:

I am a resident at 1280 Thoreau. I am writing to express my concerns and opposition regarding the proposed townhome development at Donald/Thoreau (Docket 06-21-24). Though there are many, the primary concern is the overall requirement to re-zone the parcel for the purpose of density in a historically low-density neighborhood with predominantly single-family homes.

Additionally, I would like for council/commission to help residents understand more about Hilane Realty, the developer's credentials and ability to build the proposed product that is of lasting quality. The company's website ([www.hilanerealty.com](http://www.hilanerealty.com)) is fairly generic and shows no previous development portfolio work to indicate its proven track record. Instead, the company website shows only that it owns one multi-family property in Shaker Heights, which was only acquired in July 2024 under a different LLC formed in 2017. Beyond that, the out-of-state developer appears to have more interest in return on investment rather than community development, which is evidenced by an overly dense proposal with architectural features that are uncharacteristic of the surrounding neighborhood. This is further exemplified with their website tagline of "Opportunistic Real Estate Investors." The website also reads their model is "Seeking value-add multifamily, stabilized multifamily with strong cash on cash returns, covered land / short term sale-leasebacks, and development land throughout Chicago, Cleveland, Wisconsin, Salt Lake City, and other midwestern markets."

More importantly, I echo the thoughtful critique of this project's design, density, and required site modifications provided by Mr. Kermode on June 3rd. Since, I also don't believe the second iteration of the development has significantly improved with the addition of the pedestrian path. Rather, it has shifted the driveway to my neighbor and I's shared viewshed/experiential livability including resident and service-vehicle traffic, trash cans, air conditioning noises, and general aesthetic. In closing, my primary concern and goal with this formal submission is to reinforce that the general consensus among my neighbors is not such that no development shall occur, rather it should be complementary to the surrounding neighborhood character. This proposed project is an overreach and undermines current zoning of the parcel in order to provide the density/scale the developer needs to satisfy their proforma/returns.

Thank you for your consideration.

Donny Davis

1280 Thoreau Road

330-807-3046

## Johanna Schwarz

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**From:** Dave Kermode <dwkermode@gmail.com>  
**Sent:** Thursday, November 7, 2024 9:19 AM  
**To:** Planning Dept  
**Subject:** 10-37-24 - 1286 Thoreau Road Feedback  
**Attachments:** Docket No. 10-37-24 - Kermode Adjacent Property Comment (11-7-2024 Meeting).pdf

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Good Morning,

I'm writing to submit comment for Docket No. 10-37-24 for the upcoming meeting of the Lakewood Planning Commission. My comments and concerns regarding the updated proposal can be found in the attached pdf.

Please let me know if you have any questions.

Thanks,  
Dave Kermode  
1286 Thoreau Road  
Lakewood, OH

Dave Kermode  
1286 Thoreau Road  
Lakewood, OH 44107

November 7, 2024

City of Lakewood Planning Commission  
12650 Detroit Avenue  
Lakewood, OH 44107

**Re: Docket No: 10-37-24**

Members of the Planning Commission:

I am writing in response to the upcoming final review of the proposed development intended for parcel 312-26-102. Based on the submitted plans, I have remaining concerns regarding the updated plan for the proposed development. As such, I remain strongly opposed to the plans as presented as they present specific and unique detrimental impacts to my property and existing investments made to date.

I reside at 1286 Thoreau Road in Lakewood and am the owner of the adjacent parcels immediately and contiguous to the north of the proposed development, 312-26-122 and 312-26-123, which are among, if not the most directly impacted by the decision before the commission.

The deferral granted at the October 3, 2024 meeting had the specific ask by the commission to work on a number of issues (quality of canopy trees, establishing a buffer responsive to the feedback at the meeting, etc.). In response, the applicant has now submitted revised plans that make no serious or meaningful effort to respond to these requests or overall neighborhood feedback.

I was anticipating at least a 10' buffer with landscape screening that would eclipse the height of the proposed balconies, specifically discussed during the prior meeting. The revised plans propose a 5' landscape strip with at best juniper trees that reach no more than 12' in height, not even meeting the height of the bottom support beams of the balconies at maturity. My concerns on this were also not a mystery to the applicant, as my prior submitted feedback regarding the project in August referred to a 5' strip as "wholly inadequate".

The following rendering I believe appropriately illustrates the impact of the applicants proposal, which is again, would not be legally permissible as is. The onus is on the applicant to illustrate how the project can be accomplished without specific downside harm to adjoining properties. The proposed landscaping is more akin to what one would see within a parking lot, not for any meaningful screening from adjacent residential uses.



As a potential starting point for a workable change, I suggest the following:

- Shift units #2 through #9 to the south in alignment with units #10 and #11. As shown, the limited undulation does very little to stagger the massing of the design and the land used for the setback could then be allocated to an effective buffer of at least 10' to the north
- Increased planting density with at least 80% of plantings having a maximum height of 25' or greater
- Downward facing lightning on upper balconies and light shields on the garage lights (acknowledge those are more of an ABR item)

My hope is that the commission can push the applicant/developer to answer to why requests of the commission and neighborhood feedback seem to have had no serious consideration in the revised proposal. The zoning request and near doubling of permitted density is being proposed by the applicant, not the other way around. This is the most intra-neighborhood, residential-adjacent request for a PD I believe in Lakewood to date. All other approvals had primary frontage on major roads and were either prior commercial sites, schools, or other institutional uses. This is an irregular R-2-zoned parcel bracketed by two neighborhood streets surrounded by one and two-family residential uses.

The applicant is pursuing the requested change, and the need for explanation on why the requested PD zoning change doesn't create detrimental impact, or why they should be exempt from mitigating the impacts caused by the change should fall largely to them. The begrudging nature with which the applicant has approached basic code requirements and asks of the commission, along with making few, if any changes, in light of 100 percent neighborhood opposition, has become a point of frustration. The continued bad faith and arrogance

toward project feedback by the applicant has only cost all of us additional time that we've needed to commit to consideration of the proposal.

For the previous reasons I remain strongly opposed to the final approval for the proposed development as presented and would hope for continued engagement and discussion prior to any forthcoming votes.

Thank you for your time and consideration. Please feel free to contact me with any questions.

Sincerely,  
Dave Kermode

## Johanna Schwarz

---

**From:** Katherine Herten <kbunsey@gmail.com>  
**Sent:** Friday, September 27, 2024 11:56 PM  
**To:** Planning Dept  
**Subject:** Proposed development on Thoreau

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Dear Lakewood Planning:

I am a resident and homeowner at 1347 Thoreau Road. I am also a Lakewood City Schools teacher of over 20 years. I am proud to teach, live and send my children to our schools and participate as a member in this community.

I was very disappointed to learn of the proposal to build a road, a home and a building with condos in the green space just north of the train tracks on Thoreau, across from Cove Park.

This development project threatens the safety of residents, especially children who live on Thoreau, as well as guests to Cove Park. Thoreau is already a densely populated street with limited parking for the residents in the duplex units on the south end of the street, many of whom do not even have driveways. Our street receives heavy traffic from safety and security vehicles, city vehicles and serves as a cut through for other drivers. Adding an access road to a multi resident complex near the almost 90 degree bends in Thoreau north of the tracks, poses increased safety risks as this portion of the street is already challenging for north and southbound traffic to navigate. Adding a road near this section of the street increases the potential for traffic accidents. Visibility is an issue when navigating the bends by vehicle. Usually cars slow down or pull to the side at this part of our street to allow each other to safely pass. Oftentimes on the southern portion of Thoreau, oncoming cars must pull over to wait for traffic moving in the opposite direction because of the reduced road space with completely filled street parking. Additionally, events at Cove Park such as children's recreation ball games increase the traffic and parking demands on Thoreau since the Cove parking lot cannot handle the quantity of cars for these events.

Aside from human density and driving safety issues, it is extremely disappointing that the City of Lakewood would consider developing one of our few remaining open green spaces. The addition of a multi-unit condo complex will increase light pollution for residents, which is harmful to our health. Light pollution has detrimental effects for migratory organisms as well. There are several very large/old trees near the tracks which provide habitat for owls, hawks and other birds. The area planned for development is nice clearing of grass that could be otherwise turned into vital prairie habitat for monarch butterflies, who are now declared endangered, as well as other organisms who are part of our Lake Erie ecosystem.

I beg you, Lakewood, to reconsider this plan to develop Thoreau Road for issues of safety and population/vehicle density. It is my belief that part of Keeping Lakewood Beautiful includes saving our remaining green spaces and minimizing the burden on areas that are already challenged by population density.

Thank you,  
Katherine Herten  
216-402-7496

## Johanna Schwarz

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**From:** Dave Kermode <dwkermode@gmail.com>  
**Sent:** Monday, September 30, 2024 12:45 PM  
**To:** Benjamin Kroeck  
**Cc:** Cindy A. Marx; Tom Bullock; Planning Dept; donny.davis4@gmail.com; William Fadely; debommarshall@gmail.com; jacquelyn.davis04@gmail.com; Alexis Kunsak  
**Subject:** Re: Planning Commission Meeting

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Thanks, Ben and very much appreciate your time and any consideration Councilwoman Marx and Councilman Bullock.

Following up with a Planning-specific question. I'm working on my feedback for the current submission and I can't help but notice how many missing elements are in the application. Is there any reason this wasn't held from the agenda? There's no detail on individual structures, which vary from building to building, the 3D models weren't even updated and still match the old site plan, the topo isn't shown on an actual project site plan to assess height impact by building (already two feet higher from the north in the sample structure shown versus the prelim submission), among others.

This has already been a fairly time-intensive process not on our schedule. Hopeful that this could possibly be tabled while the applicant furnishes detailed plans with adequate and necessary detail and accuracy for a hypothetical final approval review.

Thanks,  
Dave Kermode  
1286 Thoreau Road

On Mon, Sep 30, 2024 at 11:39 AM Benjamin Kroeck <[kroeckbw@gmail.com](mailto:kroeckbw@gmail.com)> wrote:

Hello Councilwoman Marx and Councilman Bullock,

Hope you've both been well. I wanted to make sure you're both aware of the Planning Commission's Meeting on Thursday evening.

Unfortunately, the latest plans (images below) have not taken into consideration any of the suggestions from the neighbors. We've asked that the road and housing be switched around so that the road would run along the railroad. The latest drawing below shows that they haven't been willing to take that into consideration. Additionally, we've asked that a larger landscape buffer be included, but this has actually been reduced to allow for now a wider two-way street. My understanding is that the buffer dimensions do not fall within the existing code. I also wonder if a street is added why it wouldn't be one-way letting out on Donald to reduce safety concerns of the nearby Cove Park entrance?

We continue to be dismayed by the lack of consideration to existing neighbors throughout this process. This is a very slippery slope that the ARB and Planning Commission have decided to take. If this rezoning is approved from Residential parcels to Planned Development then it's likely what's to come throughout Lakewood. Hoping that you'll help to facilitate more productive results within the existing code before it's too late.

Neighbors have continued to show up in good faith, but our concerns have continued to be ignored. Density and the safety concerns must also be taken into account with the nearby playground.

Hope that you'll join us Thursday evening.

Kind Regards,  
Ben Kroeck  
1237 Thoreau Road, Lakewood Ohio  
412-225-1395



VIEW AT EAST END

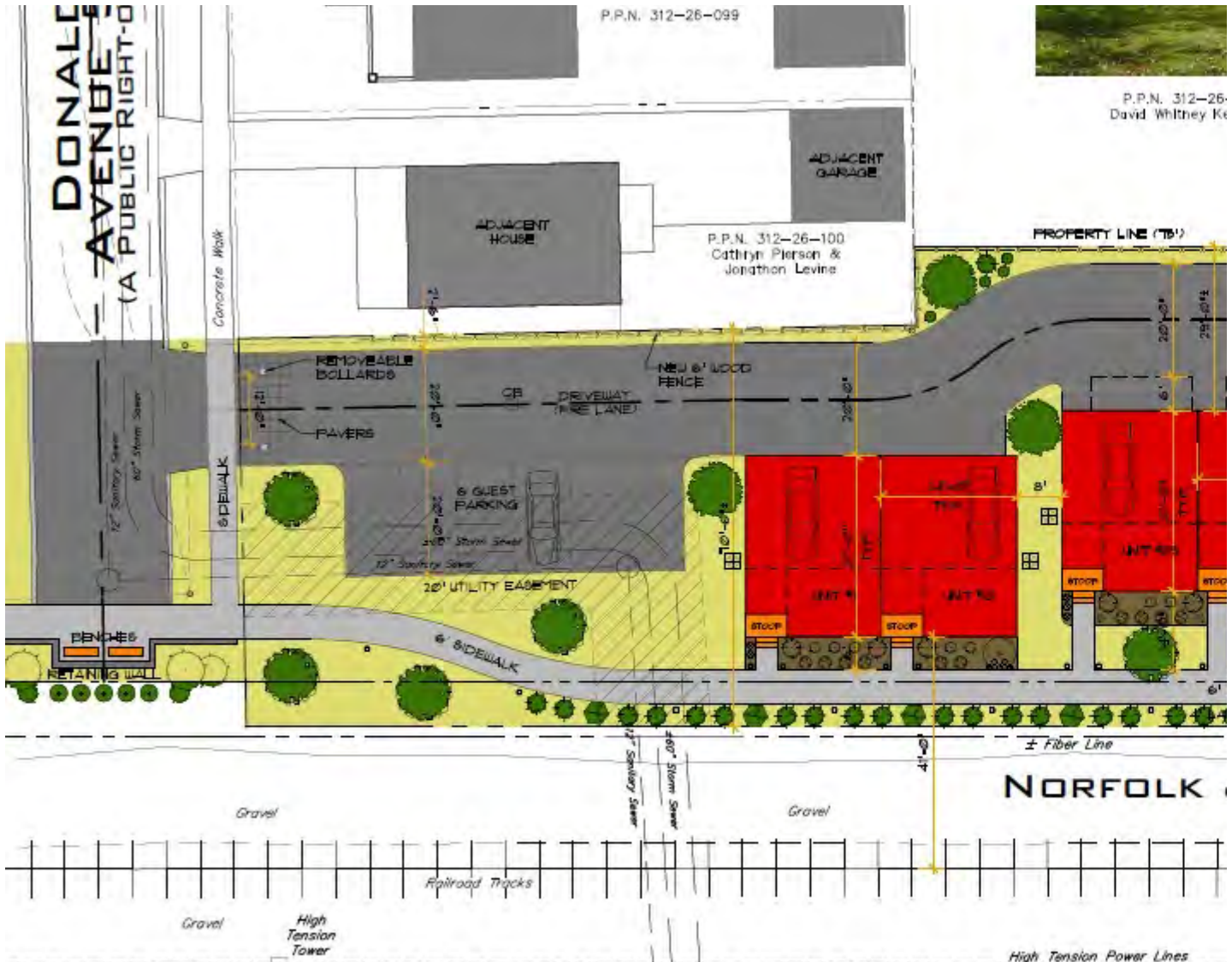
N.T.S.

**DONALD AVENUE**  
(A PUBLIC RIGHT-OF-WAY)

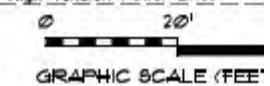
P.P.N. 312-26-099



P.P.N. 312-26-  
David Whitney Ke



**ARCHITECTURAL SITE PLAN**  
1" = 20'-0"





## VIEW ACROSS CENTER

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NT.6.

## Johanna Schwarz

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**From:** Alexis Kunsak <[alexiskunsak@gmail.com](mailto:alexiskunsak@gmail.com)>  
**Sent:** Monday, September 30, 2024 2:04 PM  
**To:** Dave Kermode  
**Cc:** Benjamin Kroeck; Cindy A. Marx; Tom Bullock; Planning Dept; [donny.davis4@gmail.com](mailto:donny.davis4@gmail.com); William Fadely; [debommarshall@gmail.com](mailto:debommarshall@gmail.com); [jacquelyn.davis04@gmail.com](mailto:jacquelyn.davis04@gmail.com)  
**Subject:** Re: Planning Commission Meeting

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For clarification, I understand that one traffic study was already conducted. However, I'd like to see another especially now that school is back in session. I ride my son to preschool nearly everyday on a bicycle to Harrison Elementary and my daughter to Emerson's kindergarten. There have been a number of very close calls that have already happened this year in the neighborhood that I have witnessed and through talking with other local families. This is a very serious issue for me.

Thanks,  
Alexis Kunsak

On Mon, Sep 30, 2024 at 1:52 PM Alexis Kunsak <[alexiskunsak@gmail.com](mailto:alexiskunsak@gmail.com)> wrote:

Dave/All,

I totally agree, in fact I was very disheartened to see that the last ARB meeting occurred in the midst of the worst power outage in over 30 years. I'd much rather see this process slow down in order to get an equitable result. As a parent who is concerned about the safety and visibility of all pedestrians entering the park, and particularly children headed to the playground and to Lakewood Family Room outdoor programming, I'd like to see a traffic study or a careful evaluation of the proximity of the private road to the blind curve in Thoreau Rd. In fact, it might be worth considering a speed table to help further slow traffic as these are becoming more prevalent across Lakewood and really seem to help. Overall I feel that residential housing would be a better fit for the neighborhood.

Thanks,  
Alexis Kunsak  
1237 Thoreau Road

On Mon, Sep 30, 2024 at 12:45 PM Dave Kermode <[dwkermode@gmail.com](mailto:dwkermode@gmail.com)> wrote:

Thanks, Ben and very much appreciate your time and any consideration Councilwoman Marx and Councilman Bullock.

Following up with a Planning-specific question. I'm working on my feedback for the current submission and I can't help but notice how many missing elements are in the application. Is there any reason this wasn't held from the agenda? There's no detail on individual structures, which vary from building to building, the 3D models weren't even updated and still match the old site plan, the topo isn't shown on an actual project site plan to assess height impact by building (already two feet higher from the north in the sample structure shown versus the prelim submission), among others.

This has already been a fairly time-intensive process not on our schedule. Hopeful that this could possibly be tabled while the applicant furnishes detailed plans with adequate and necessary detail and accuracy for a hypothetical final approval review.

Thanks,  
Dave Kermode  
1286 Thoreau Road

On Mon, Sep 30, 2024 at 11:39 AM Benjamin Kroeck <[kroeckbw@gmail.com](mailto:kroeckbw@gmail.com)> wrote:

Hello Councilwoman Marx and Councilman Bullock,

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We continue to be dismayed by the lack of consideration to existing neighbors throughout this process. This is a very slippery slope that the ARB and Planning Commission have decided to take. If this rezoning is approved from Residential parcels to Planned Development then it's likely what's to come throughout Lakewood. Hoping that you'll help to facilitate more productive results within the existing code before it's too late.

Neighbors have continued to show up in good faith, but our concerns have continued to be ignored. Density and the safety concerns must also be taken into account with the nearby playground.

Hope that you'll join us Thursday evening.

Kind Regards,  
Ben Kroeck  
1237 Thoreau Road, Lakewood Ohio  
412-225-1395



VIEW AT EAST END

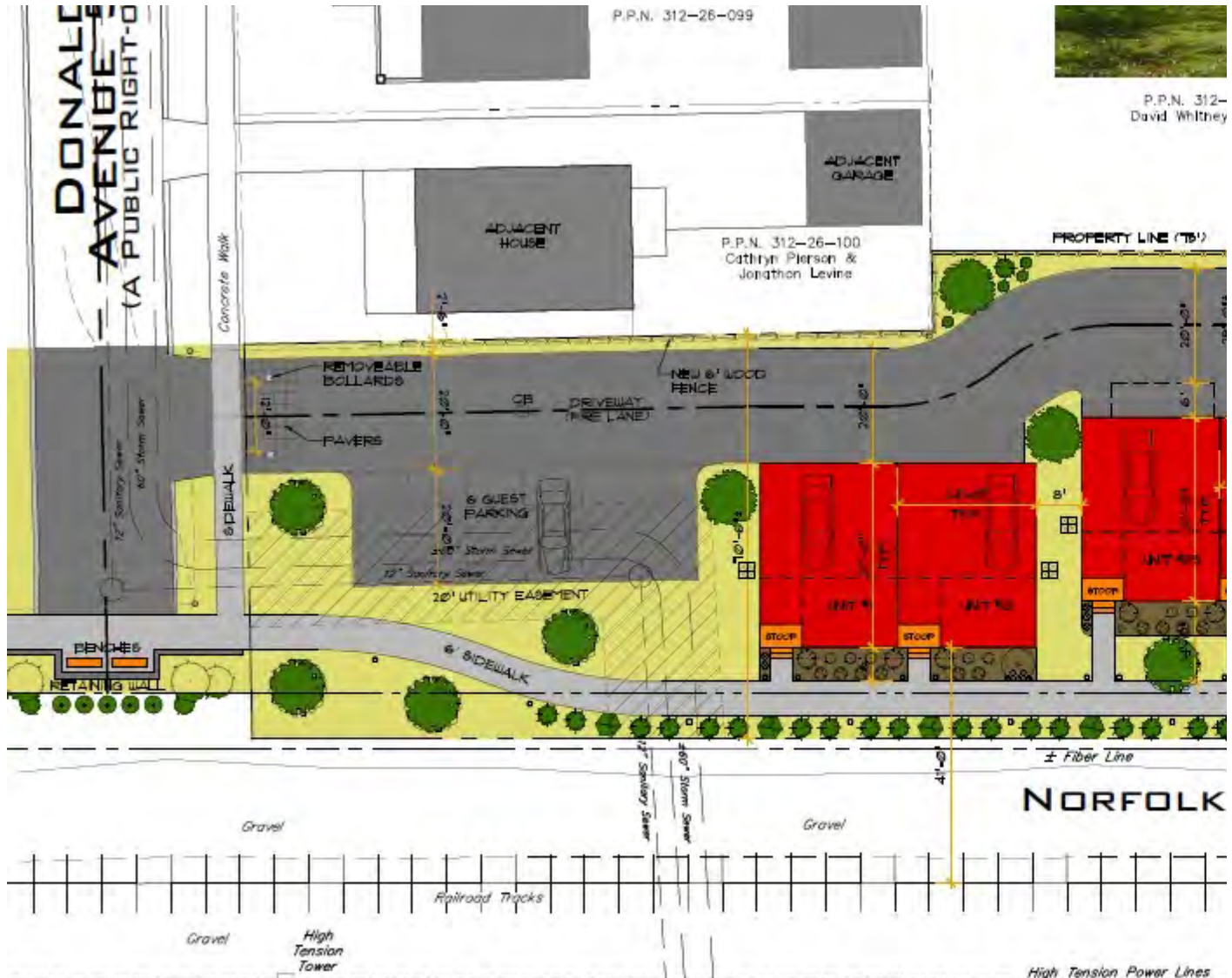
N.T.S.

**DONALD  
AVENUE**  
(A PUBLIC RIGHT-OF-WAY)

P.P.N. 312-26-099

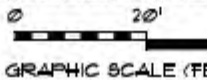


P.P.N. 312-  
David Whitney



**ARCHITECTURAL SITE PLAN**

1" = 20'-0"





## VIEW ACROSS CENTER

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NT.5.

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Alexis Kunsak  
216.526.4901  
[alexiskunsak@gmail.com](mailto:alexiskunsak@gmail.com)

--

Alexis Kunsak  
216.526.4901  
[alexiskunsak@gmail.com](mailto:alexiskunsak@gmail.com)

## Johanna Schwarz

---

**From:** Dave Kermode <dwkermode@gmail.com>  
**Sent:** Wednesday, October 2, 2024 3:51 PM  
**To:** David Baas  
**Cc:** Planning Dept; Amanda L. Cramer; Angela J. Byington  
**Subject:** Re: Planning Commission Meeting  
**Attachments:** Docket No. 10-37-24 - Kermode Adjacent Property Comment (10-3-2024 Meeting).pdf

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Thanks, I appreciate it. Please find attached my fuller comments for the upcoming meeting. Let me know if there are any questions.

Thanks,  
Dave Kermode  
1286 Thoreau Road

On Mon, Sep 30, 2024 at 3:01 PM David Baas <[David.Baas@lakewoodoh.gov](mailto:David.Baas@lakewoodoh.gov)> wrote:

Mr. Kermode,

Thank you for the email – it will be forwarded to the Planning Commission for their consideration and included with the record for this item.

Sincerely,

Dave Baas

**Dave Baas, AICP**

Assistant Director, Planning & Development

LtCol, USMC (Retired)

City of Lakewood

12650 Detroit Avenue

Lakewood, Ohio 44107

[david.baas@lakewoodoh.gov](mailto:david.baas@lakewoodoh.gov)

(216) 529-6637 (work)

(216) 372-8996 (cell)

[www.lakewoodoh.gov](http://www.lakewoodoh.gov)

[Sign up for the City's e-Newsletter](#)



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**From:** Dave Kermode <[dwkermode@gmail.com](mailto:dwkermode@gmail.com)>

**Sent:** Monday, September 30, 2024 12:45 PM

**To:** Benjamin Kroeck <[kroeckbw@gmail.com](mailto:kroeckbw@gmail.com)>

**Cc:** Cindy A. Marx <[cindy.marx@lakewoodoh.gov](mailto:cindy.marx@lakewoodoh.gov)>; Tom Bullock <[Tom.Bullock@lakewoodoh.gov](mailto:Tom.Bullock@lakewoodoh.gov)>; Planning Dept <[planning@lakewoodoh.gov](mailto:planning@lakewoodoh.gov)>; [donny.davis4@gmail.com](mailto:donny.davis4@gmail.com); William Fadely <[willfade3@gmail.com](mailto:willfade3@gmail.com)>; [debommarshall@gmail.com](mailto:debommarshall@gmail.com); [jacquelyn.davis04@gmail.com](mailto:jacquelyn.davis04@gmail.com); Alexis Kunsak <[alexiskunsak@gmail.com](mailto:alexiskunsak@gmail.com)>

**Subject:** Re: Planning Commission Meeting

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Dave Kermode

1286 Thoreau Road

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Kind Regards,  
Ben Kroeck

1237 Thoreau Road, Lakewood Ohio

412-225-1395



VIEW AT EAST END

N.T.S.

**DONALD  
AVENUE**  
(A PUBLIC RIGHT-OF-WAY)

P.P.N. 312-26-099



P.P.N. 312-2  
David Whitney I

ADJACENT GARAGE

ADJACENT HOUSE

P.P.N. 312-26-100  
Cathryn Pierson &  
Jonathon Levine

PROPERTY LINE (75')

Concrete Walk

REMOVABLE BOLLARDS

PAVERS

CE DRIVEWAY (PER LANE)

NEW WOOD FENCE

8 GUEST PARKING

2x8 Storm Shelter

20' UTILITY EASEMENT

SIDEWALK

SIDEWALK

± Fiber Line

**NORFOLK**

Gravel

Gravel

Railroad Tracks

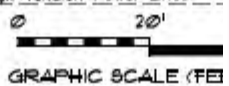
Gravel

High Tension Tower

High Tension Power Lines

# ARCHITECTURAL SITE PLAN

1" = 20'-0"





VIEW ACROSS CENTER

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N.T.S.

Dave Kermode  
1286 Thoreau Road  
Lakewood, OH 44107

October 2, 2024

City of Lakewood Planning Commission  
12650 Detroit Avenue  
Lakewood, OH 44107

**Re: Docket No: 10-37-24**

Members of the Planning Commission:

I am writing in response to the upcoming final review of the proposed development intended for parcel 312-26-102. Based on the submitted plans, I have several new and remaining concerns regarding the updated conceptual plan for the proposed development. As such, I remain strongly opposed to the plans as presented as they present specific and unique detrimental impacts to my property and existing investments made to date.

I reside at 1286 Thoreau Road in Lakewood and am the owner of the adjacent parcels immediately and contiguous to the north of the proposed development, 312-26-122 and 312-26-123, which are among, if the most directly affected by the decision before you, especially given what appears to be bad faith engagement with adjacent owners. It inspires confidence in neither good outcomes on the process to date and

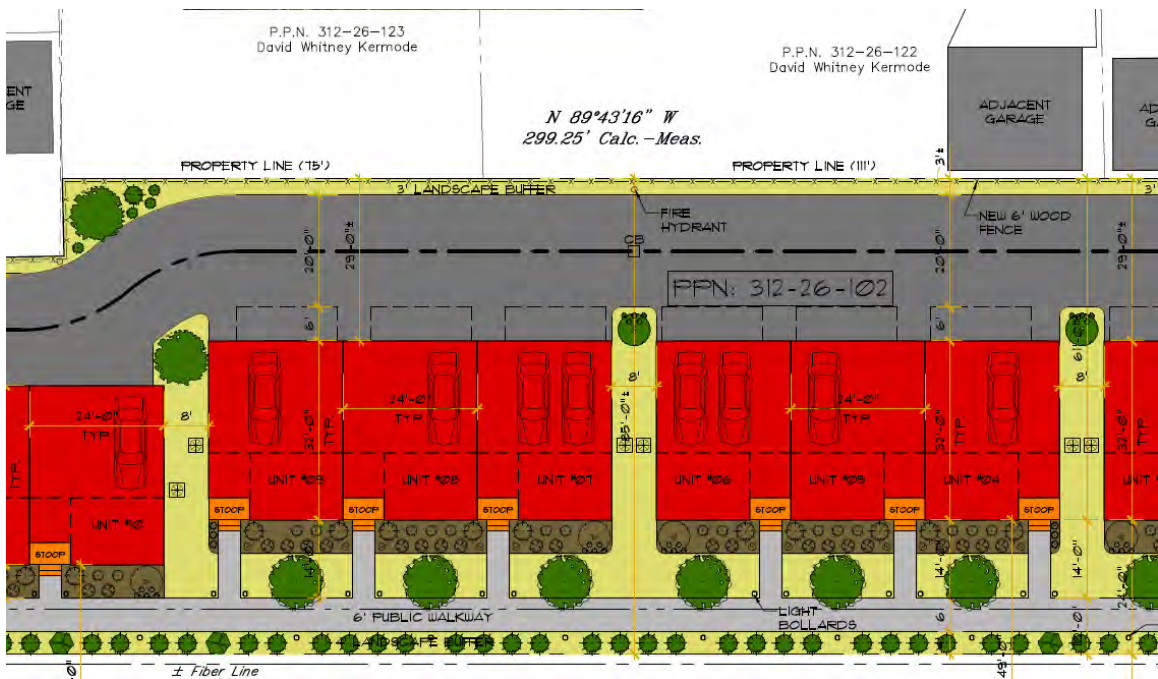
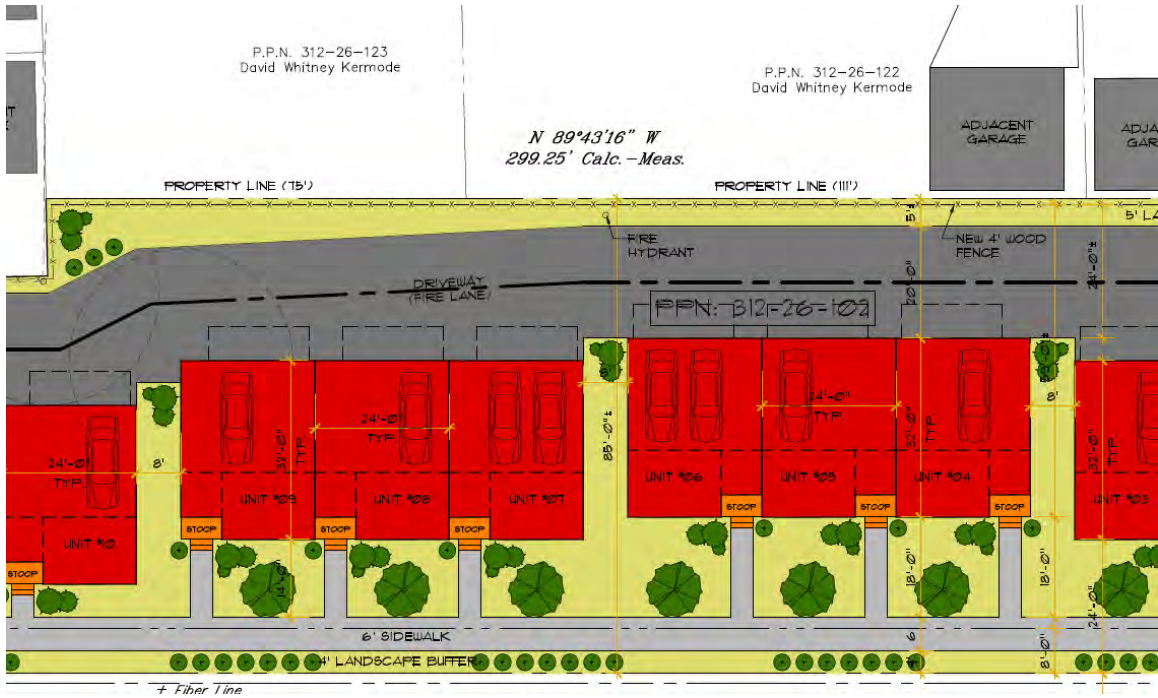
At the previous meeting, it was clearly directed to the applicant to work with adjacent property owners. There was subsequently no contact from either the applicant/developer or anyone with the city to facilitate said engagement. As a result, it required a Thoreau neighbor to attempt proactive contact with the applicant/developer on August 18<sup>th</sup>. This yielded a follow up request to meet from the applicant/developer on August 21<sup>st</sup> which we quickly made an effort to confirm and arrange. The meeting was very much in good faith on our part, and in fact led to some optimism afterwards it could be the start of a better process moving forward.

When opening the latest set of plans, it appeared to specifically make worse clearly conveyed areas of concern including the width of the buffer (5' to 11' as of the plan prior to the meeting, now reduced to 3' without any landscaping apart from grass seed) which was requested to be widened to account for spillover impacts of road salting on neighboring landscape that's more heavily impacted, along with vertical planting in excess of the fence height to mitigate the elevated balconies that have very clear viewsheds over the majority of my home and yard, including my own landscaping. That was left out entirely.

My hope is that the commission can push the applicant/developer to speak to how neither of those items appear to be considered whatsoever despite good faith efforts to make time available on the applicants schedule and location that simply nothing was done as a result.

My remaining feedback/comments follows:

- My submitted feedback has noted concerns regarding the interface between the property boundaries. My previous comment noted that the approximately 5' to 11' foot (running from east to west) boundary was inadequate. The revised plan now reduced this further to 3'. I would hope the commission could more forcefully push back on the applicant on that point and good faith efforts to engage with contiguous property owners on material and detrimental impacts created solely by the development as proposed. The following shows the evolution of the plan despite this specific issue being raised with the applicant.



- The proposed three foot grass strip along the northern property line adjacent to the proposed 20' to 26' wide through roadway is even more wholly inadequate for any meaningful screening. Further, the plans as presented appear to lack any planned landscaping apart from a six foot fence and grass seed for the strip between the proposed roadway and the northern property line.
- A six foot fence alone (unless combined with landscaping that eclipses the height by at least twice the height of the fence) won't serve as any impactful screening from north facing building lighting (including proposed coach lights and upper floor balcony lights with no effort at screening or shielding of lights), headlights and taillights from the 11 north facing garage bays, and general vehicle traffic from a new east/west 20' to 26' wide through roadway from Donald Avenue to Thoreau Road.
- The limited setback and close proximity of the proposed roadway needlessly places the onus of any required screening on adjacent property owners.
- With the introduction of vehicle access for 11 new residences, including waste removal and service/seasonal plowing vehicles, introduces wintertime road salt to already installed landscaping by adjacent owners. Further, with litigation concerns of contract landscape companies, oversalting is a very common occurrence for residential developments overseen by a homeowners association.
- The further narrowing of the strip from the prior plan now renders this a more significant issue as it bring the parking, loading and circulation infrastructure and plowing needs closer to the property line with detrimental and demonstrable impacts on the plants I have along the border (see addendum with replacement mature tree pricing and information regarding salt sensitivity for both ground intrusion and any salt/airborne salt due to drier winter conditions). Norway Spruce are notably sensitive to road salt, which are the largest concentration (12 total) of trees along the southern border of my parcels, which would only present due to an overwide parking, loading and circulation infrastructure that would not have been legally permissible nor was existing when the landscaping was installed.
- ***Additionally, when the parking, loading and circulation infrastructure was moved to the north it seems apparent that this section of the PD code should be applied. I would expect more robust discussion of why city staff and the applicant felt no need to take these steps, including offering of more robust masonry screening or discussion of suitably design alternatives, despite clear directive in the code and/or why this should not be a direct and enforced result of the process.***

(f) Landscaping and Screening.

(1) It shall be the duty of the Commission to determine whether a reasonable percentage of the area within a PD shall be maintained in a combination of landscaped and urban open space. The project must adhere to the spirit of the City's landscape values. The standards contained in Chapter 1141 shall be used as a guide, but those standards may be modified without the need for a variance based upon other considerations determined by the Commission that such considerations would comply with the provisions of the Code and the intent of the PD.

(2) Pedestrian access from adjacent residential streets is encouraged. The owners of residential property directly abutting rear yards, parking and loading areas of a PD shall be contacted and offered masonry screening and/or appropriately designed alternatives. PD applicants shall document meetings with abutting residential owners and the results of such meetings. The intent of this provision is for the applicant to involve nearby residents in the PD project. City staff shall assist in this process.

(3) Where required, screening fences and walls shall be erected. The standards of Chapter 1141 shall be used as a guide, but those standards may be modified without the need for a variance based upon other considerations determined by the Commission that such considerations would comply with the provisions of the Code and the intent of the PD.

(4) If used, the owner of the property on which the fence is required to be erected shall permanently and adequately maintain screening fences or walls. The following types of walls or fences shall qualify, Chapter 1141 notwithstanding.

(5) Masonry wall or fence. Masonry fences or walls shall be constructed with the finish side out and of any of the following materials:

- A. Native stone
- B. Brick
- C. Precast concrete panels with decorative finish or decorative concrete masonry units
- D. In no case shall more than twenty-five percent (25%) of the area of the fence be erected with common smooth-face masonry units.

(6) Ribbed metal panel fence. Suitably finished to blend with the primary structure and supported by structurally sound metal frame.

(7) Vegetative screening. Using plants and fence materials, vegetative screens may be proposed.

(8) Screening of roof-mounted equipment. All roof-mounted equipment that rises above the roofline of any building or structure

- The proposed development as designed continues to run counter to the reported intent of the Planned Development guidelines from the City of Lakewood Planning and Zoning Code, which states (emphasis mine):

*1156.01 Purpose - (a) Planned Development (PD) zoning is intended to encourage orderly development and redevelopment of property, while allowing more flexibility and creativity in design to achieve high quality, integrated site planning not otherwise possible under the constraints of normal zoning requirements **without detriment to neighboring properties.***

*1156.02 Location of Planned Developments - (c) Grouping of uses permitted in other districts to create developments of compatible and mutually supportive activities is encouraged **provided there are adequate buffers to adjacent properties of other uses and designs to promote compatibility and transitions to adjacent properties.***

For the previous reasons I remain strongly opposed to the final approval for the proposed development as presented and would hope for far more robust engagement and discussion prior to any forthcoming votes.

Thank you for your time and consideration. Please feel free to contact me with any questions.

Sincerely,  
Dave Kermode



**Size**

12ft

**\$1,675.00**

- 1 + **ADD TO CART**

Categories: Evergreen Trees, Spruce

**BIOCHAR ENHANCED VERMICASTING** **\$35.00**  
- 1 + **ADD TO CART**

**DEEP-ROOT FERTILIZATION & EXTENDED WARRANTY** **\$110.00**  
- 1 + **ADD TO CART**

Be sure to adjust the quantity to match your tree order.

Cost of damage to 12 existing Norway Spruce approximately 12' in height\*:  $\$1,675 \times 12 = \$20,100$   
\*Existing Norway Spruce trees exceed this height currently, but landscapers generally will avoid installing anything larger due to transplant shock



**Size**

15ft

**\$1,250.00**

- 1 + **ADD TO CART**

Categories: Birch, Shade Trees

**BIOCHAR ENHANCED VERMICASTING** **\$35.00**  
- 1 + **ADD TO CART**

**DEEP-ROOT FERTILIZATION & EXTENDED WARRANTY** **\$110.00**  
- 1 + **ADD TO CART**

Cost of damage to five existing Paper Birch approximately 15' in height:  $\$1,250 \times 5 = \$6,250$

# Salt Damage in Landscape Plants

*Janna Beckerman*  
Department of Botany and  
Plant Pathology  
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**I**n the upper Midwest, each state applies approximately 100,000 to 300,000 tons of de-icing salt to roads each winter. And nationwide each year, we apply more than 15 million tons to de-ice sidewalks, walkways, and driveways.

As salt dissolves and spreads to adjacent soil it is taken up by plant roots. Plants vary in their sensitivity to salt, with some plants seemingly unaffected by salt while others are killed outright (Figure 1). This publication examines the causes of salt damage in plants and offers some tips for reducing its risks.

## What Is Salt?

The most commonly used salt, whether found in the pantry or in a de-icing truck, is sodium chloride. Salt occurs in a variety of forms, including the mineral halite, which is mined and used in rock salt.

Sodium chloride is sold in several different particle sizes depending on its intended use. Rock salt is very coarse and consists of discrete crystals that have the consistency of loose gravel. On the opposite end of the scale, common table salt and popcorn salt have very fine granules. In between are kosher salt, which is made up of coarse flakes, and compressed pellets that are used in water softeners. Road de-icers often consist of both salt and sand, with the salt component consisting mostly (98.5 percent) of common sodium chloride with traces of other mineral salts.

## How Salt Damages Plants

Salt's toxic effects on plants have been known since ancient times when it was used for biological warfare to destroy an enemy's fields and crops. Common salt is



Photo by Janna Beckerman

**Figure 1.** Different plants have different sensitivity to salt. The leaves from this oak tree show telltale signs of extreme salt damage.



Photo by Janna Beckerman

**Figure 2.** The symptoms of salt damage are often more severe where plants face roads or sidewalks — as with these yews.

toxic because it is made up of sodium and chloride and both are toxic to plants when present in high concentration.

When salt dissolves in water, the sodium and chloride ions separate. When this happens, the sodium ions in the salt replace the other nutrients in the soil that plants need (potassium, calcium, and magnesium), so these nutrients are unavailable to the plant. Rock salt also absorbs the water that would normally be available to roots, which dehydrates the roots, changes their physiology, and causes additional plant stress. Meanwhile, roots absorb the chloride ions and transport them to the leaves, where they accumulate and interfere with chlorophyll production and photosynthesis.

One study found that the soil from a highway median had a chloride concentration of 1,050 parts per million (ppm) (Hofstra and Smith, 1984). The soil less than 30 feet from the highway had a chloride concentration of 890 ppm, which is still ten times greater than levels that are known to inhibit seed germination and root growth in grasses and wildflowers. Such salty soil is one reason the salt-loving and invasive giant reed *Phragmites* can colonize sites near many Midwest roads.

Salt damage doesn't stop with the roots. When passing vehicles spray salt on plants, it can damage a plant's leaves, buds, and small twigs, which in turn can reduce the plant's cold hardiness, making tissue more susceptible to freeze damage.

### How to Diagnose Salt Damage

The amount and duration of salt exposure directly affects the potential damage to plants. As you may expect, higher concentrations of salt in the soil cause more damage.

To determine if salt is playing a role in damaging your plants, be sure to note which side of the plant has more severe symptoms. In salt-damaged plants, the symptoms will be more severe on sides facing the road or sidewalk (Figure 2). In evergreens, damage usually appears in late winter as needle browning that starts at the tips. Keep in mind that snow-covered branches will be less affected than those exposed to salt spray, and that as you move away from the spray zone, the symptoms should abate.

It is more difficult to diagnose spray damage on deciduous plants. Usually, leaf buds facing the road are killed or are very slow to break dormancy and bud and leaf out in spring. Flower buds facing the road often fail, but the unaffected side of the tree or shrub flowers normally. Repeated salt damage over several winters may produce a witch's broom effect, which is a tufted and stunted appearance of the plants on the side facing the road.

### Salt Alternatives and How to Prevent Salt Damage

The easiest way to prevent salt damage is to avoid it. Whenever possible, use coarse sand instead of salt to provide traction and make sidewalks and driveways less slick. If you must use salt, use it judiciously, and erect barriers with plastic fencing, burlap, or snow fencing to protect sensitive plants and minimize their contact with salt (Figure 3).

When possible, consider non-sodium de-icing agents such as calcium chloride or calcium magnesium acetate (CMA), a salt-free melting agent made from limestone and acetic acid. The relative costs of these products are provided in Table 1. There



Photo by Janna Beckerman

**Figure 3.** Covering plants especially sensitive to salt with burlap or other materials can reduce damage caused by salt spray from roads.

are simple practices that homeowners can implement when plants are grown in areas that receive heavy salt applications. Keep in mind that plants are highly adaptive and vary in their ability to grow in salty soils.

Plants that tolerate high-salt soils are referred to as “halophytic” or salt loving. These plants naturally occur by ocean coastlines, estuaries, and salt-water marshes. By utilizing more salt-tolerant plants, salt damage may be minimized or avoided. Table 2 lists salt-tolerant plants hardy for Indiana. But remember: severe salt can still damage or kill even salt-tolerant plants — the dose makes the poison!

In those years when large amounts of salt are used, minimize plant damage by irrigating soils to leach out the sodium and

chloride prior to spring growth. Since most salts are water-soluble, thorough and repeated applications of water can effectively leach salts out of the root zones

Salt damage can be deadly when excessive or repeated exposure occurs with a salt-sensitive plant. Simple preventative measures can help prevent salt damage or plant death. These measures include avoiding salt-sensitive plants in high-traffic areas, protecting plants (salt sensitive and salt tolerant) with barriers, using sand to de-ice rather than salt, using formulations of salt that don't contain sodium chloride, and irrigating soils deeply if sensitive plants were repeatedly exposed to salt.

**Table 1. De-icing Alternatives**

De-icing Product	Characteristics
Calcium Chloride (CaCl <sub>2</sub> )	<ul style="list-style-type: none"> <li>• Melts ice at temperatures to -25°F</li> <li>• Effective to -59°F</li> <li>• If used as recommended, will not harm vegetation</li> <li>• More expensive than sodium chloride</li> </ul>
Calcium Magnesium Acetate (CMA)	<ul style="list-style-type: none"> <li>• Will work below 0°F</li> <li>• Provides needed nutrients (Ca, Mg) for plants</li> <li>• Low toxicity and biodegradable</li> <li>• Most expensive</li> </ul>
Magnesium Chloride (MgCl <sub>2</sub> )	<ul style="list-style-type: none"> <li>• Lowest practical temperature: 5°F</li> <li>• Will not harm vegetation, if used as recommended; however, MgCl<sub>2</sub>, on a percentage basis, contains 17-56% more chloride ion than other “salt”-type deicers</li> <li>• More expensive than sodium chloride</li> </ul>
Potassium Chloride (KCl)	<ul style="list-style-type: none"> <li>• Lowest practical temperature: 12°F</li> <li>• Will not harm vegetation</li> <li>• More expensive than sodium chloride</li> </ul>
Sodium Chloride (NaCl)	<ul style="list-style-type: none"> <li>• Effective to 16°F</li> <li>• Can soften ice at lower temperatures</li> <li>• Inexpensive and effective, but damaging to roadside plants due to sodium and chloride ion toxicity</li> <li>• Least expensive de-icing product</li> </ul>
Urea	<ul style="list-style-type: none"> <li>• Lowest practical temperature: 15°F</li> <li>• Potential harm to vegetation</li> <li>• Could cause nitrogen pollution to ponds and waterways</li> <li>• More expensive than sodium chloride</li> </ul>

**Source:** Stormwater: The Journal for Surface Water Quality Professionals.

**Table 2.** Landscape Plants with Reported Salt Tolerance

When selecting plants for residential, city, or highway plantings, keep salt tolerance in mind. This table provides the reported salt tolerances of selected trees, shrubs, and vines. The tolerances are for resistance to salt spray, soilborne salt, and salt from other sources (including uncommon or rare sources such as naturally saline water and salts from home plumbing).

Choose your plants carefully, and remember that the degree of injury your plants could suffer depends on many variables. Salt tolerance can vary within the same species, so the information provided here is a relative judgment — there are even

conflicting reports about many species. Also, data recorded from different climates or soil types may have questionable application in other areas.

**Key**

T = tolerant

M = intermediate

S = sensitive

— = No information available

\* = invasive, not recommended in Indiana

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
<i>Abies balsamea</i> Balsam Fir	M	S	M, S
<i>Abies concolor</i> White Fir	T	—	—
<i>Acer campestre</i> Hedge Maple	T, M	—	—
<i>Acer ginnala</i> Amur Maple	M, S	—	M
<i>Acer negundo</i> Box-elder	M, S	M	M, S
<i>Acer palmatum</i> Japanese Maple	S	—	—
<i>Acer pseudoplatanus</i> Sycamore Maple	T	S	S
<i>Acer rubrum</i> Red Maple	M, S	S	S
* <i>Acer saccharinum</i> Silver Maple (native invasive, not recommended)	T, M	S	M, S
<i>Acer tataricum</i> Tatarian Maple	S	—	S
<i>Aesculus hippocastanum</i> Horse-Chestnut	T	T	—
<i>Alnus incana</i> Speckled Alder, White Alder	M	S	S
<i>Alnus rugosa</i> Speckled Alder, Hazel Alder	M	—	S
<i>Amelanchier x grandiflora</i> Apple Serviceberry	S	—	—
<i>Amelanchier laevis</i> Alleghany Serviceberry	S	—	—
* <i>Berberis</i> species Barberry	M, S	S	S
<i>Betula alleghaniensis</i> Yellow Birch	—	—	T
<i>Betula davurica</i> Dahurian Birch	—	—	S
<i>Betula lenta</i> Cherry Birch, Sweet Birch	—	—	T

Table 2. Continued

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
<i>Betula nigra</i> River Birch	—	S	—
<i>Betula papyrifera</i> Canoe Birch, Paper Birch	M	—	T, M, S
<i>Betula pendula</i> European White Birch	M	—	—
<i>Betula populifolia</i> Gray Birch	M	—	T
<i>Buxus sempervirens</i> Common Boxwood	—	S	S
<i>Caragana arborescens</i> Siberian Pea-shrub	T	—	T
<i>Carpinus betulus</i> European Hornbeam	S	S	S
<i>Carpinus caroliniana</i> American Hornbeam, Blue Beech	—	—	S
<i>Carya glabra</i> Pignut Hickory	S	T, S	—
<i>Carya ovata</i> Shagbark Hickory	T, M	—	S
<i>Carya</i> species Hickory	—	—	S
<i>Catalpa speciosa</i> Northern or Western Catalpa	M	—	—
<i>Celtis occidentalis</i> Hackberry	S	—	M, S
<i>Cercis canadensis</i> Eastern Redbud	S	—	—
<i>Chaenomeles speciosa</i> Flowering-Quince	M, S	—	—
<i>Chamaecyparis pisifera</i> Sawara False-cypress	S	—	—
<i>Cladrastis lutea</i> American Yellowwood	M	—	—
<i>Cornus alba</i> Tartarian Dogwood	S	—	—
<i>Cornus mas</i> Cornelian-Cherry	S	—	—
<i>Cornus racemosa</i> Gray Dogwood	S	—	—
<i>Cornus sericea</i> ( <i>Cornus stolonifera</i> ) Red Osier Dogwood	S	—	S
<i>Corylus avellana</i> European Filbert	S	S	S
<i>Crataegus crus-galli</i> Cockspur Hawthorn	S	—	S
<i>Crataegus laevigata</i> ( <i>Crataegus oxyacantha</i> ) English Hawthorn	S	—	—

**Key**

T = tolerant    M = intermediate    S = sensitive    — = No information available    \* = invasive, not recommended in Indiana

Table 2. Continued

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
<i>Crataegus</i> species Hawthorn	M, S	M	T
<i>Euonymus europaea</i> European Spindletree	S	—	—
<i>Fagus grandifolia</i> American Beech	M, S	—	S
<i>Fagus sylvatica</i> European Beech	S	S	S
<i>Forsythia x intermedia</i> Showy Border Forsythia	M	—	—
<i>Fraxinus americana</i> White Ash (not recommended due to emerald ash borer)	M	S	T, M
<i>Fraxinus excelsior</i> European Ash (not recommended due to emerald ash borer)	T	—	—
<i>Fraxinus pennsylvanica</i> Green Ash (not recommended due to ash emerald borer)	M	T, M	T, M
<i>Ginkgo biloba</i> Maidenhair Tree	M	—	—
<i>Gleditsia triacanthos</i> Honey-Locust	T, S	T	T
<i>Gymnocladus dioica</i> Kentucky Coffee Tree	T	—	—
<i>Halimodendron halodendron</i> Salt Tree	T	—	—
<i>Hippophae rhamnoides</i> Sea-buckthorn	T, M	T	T
<i>Juglans nigra</i> Black Walnut	M	S	S
<i>Juglans regia</i> Carpathian Walnut, English Walnut	M	—	—
<i>Juniperus chinensis</i> 'Pfitzerana' Pfitzer Juniper	—	T	—
<i>Juniperus horizontalis</i> 'Plumosa' Andorra Juniper	—	T	—
<i>Juniperus</i> species Juniper	T, M	—	—
<i>Juniperus virginiana</i> Eastern Red-cedar	T, M	M	T, M
<i>Kolkwitzia amabilis</i> Beauty Bush	S	—	—
<i>Larix decidua</i> European Larch	T	—	—
<i>Larix</i> species Larch	T	—	S

**Key**

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Table 2. Continued

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
* <i>Ligustrum</i> species Privet	M, S	—	—
* <i>Ligustrum vulgare</i> Common Privet	M, S	S	S
<i>Liriodendron tulipifera</i> Tulip Tree, Yellow-poplar	S	—	—
* <i>Lonicera</i> species Honeysuckle (some species are invasive and not recommended)	S	—	—
<i>Lycium</i> species Matrimony Vine	T	T	T
<i>Malus baccata</i> Siberian Crabapple	—	—	M
<i>Malus</i> species and cultivars Apple, Crabapple	S	—	M, S
<i>Metasequoia glyptostroboides</i> Dawn Redwood	S	—	—
<i>Parthenocissus quinquefolia</i> Virginia Creeper, Woodbine	T	—	T
<i>Physocarpus opulifolius</i> var. <i>intermedius</i> Dwarf Eastern Ninebark	—	—	M, S
<i>Picea abies</i> Norway Spruce	M, S	S	—
<i>Picea glauca</i> White Spruce	T, S	M	M, S
<i>Picea glauca</i> 'Densata' Black Hills Spruce	—	—	T
<i>Picea pungens</i> Colorado Spruce	—	M, S	—
<i>Picea pungens</i> 'Glauca' Blue Colorado Spruce	T	—	T, M
<i>Pinus banksiana</i> Jack Pine	T	—	—
<i>Pinus cembra</i> Swiss Stone Pine	S	—	—
<i>Pinus mugo</i> Mugho Pine	T	T	—
<i>Pinus nigra</i> Austrian Pine, Black Pine	T	—	T, M
<i>Pinus ponderosa</i> Ponderosa Pine	—	M	T, M
<i>Pinus resinosa</i> Norway Pine, Red Pine	S	S	S
<i>Pinus strobus</i> Eastern White Pine	S	S	S
<i>Pinus sylvestris</i> Scotch Pine	M, S	—	T, M, S
<i>Pinus thunbergiana</i> ( <i>Pinus thunbergii</i> ) Japanese Black Pine	T	—	—

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Table 2. Continued

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
<i>Platanus x acerifolia</i> London Plane Tree	S	—	—
<i>Platanus occidentalis</i> American Sycamore	S	—	—
<i>Populus canescens</i> Gray Poplar	T	T	T
* <i>Populus deltoides</i> Eastern Cottonwood (native invasive, not recommended)	T	T, S	T, M, S
<i>Populus grandidentata</i> Bigtooth Aspen	T, M	—	T
<i>Populus laurifolia</i> Laurel Poplar	—	S	S
<i>Populus nigra</i> 'Italica' Lombardy Poplar	T, M	S	T, S
<i>Populus tremuloides</i> Quaking Aspen	T, M	T	T, M, S
<i>Potentilla fruticosa</i> 'Jackmanii' Jackman Shrubby Cinquefoil	—	—	T
<i>Prunus armeniaca</i> Apricot	—	—	T
<i>Prunus avium</i> Mazzard Cherry	M	—	—
<i>Prunus padus</i> European Bird Cherry	T, M	T	—
<i>Prunus persica</i> Peach	S	—	—
<i>Prunus serotina</i> Black Cherry	S	—	T, S
<i>Prunus serrulata</i> 'Kwanzan' Kwanzan Flowering Cherry	S	—	—
<i>Prunus virginiana</i> Chokecherry	T, M	—	—
<i>Pseudotsuga menziesii</i> Douglas-fir	M, S	M, S	M, S
<i>Pyracantha coccinea</i> Scarlet Firethorn	S	—	M
<i>Pyrus</i> species Pear	T, M	—	—
<i>Quercus alba</i> White Oak	M, S	T	T
<i>Quercus bicolor</i> Swamp White Oak	S	—	—
<i>Quercus coccinea</i> Scarlet Oak	S	—	—
<i>Quercus macrocarpa</i> Bur Oak	M	T	T, M
<i>Quercus muehlenbergii</i> Chinkapin Oak, Yellow Chestnut Oak	S	—	—
<i>Quercus palustris</i> Pin Oak	S	—	S

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Table 2. Continued

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
<i>Quercus robur</i> English Oak	S	T	T
<i>Quercus rubra</i> Northern Red Oak	M, S	T	T
<i>Quercus velutina</i> Black Oak	—	T	—
<i>Rhus glabra</i> Smooth Sumac	—	—	M
<i>Rhus trilobata</i> Skunkbush, Squawbush	—	T	T
<i>Rhus typhina</i> Staghorn Sumac	T	—	—
<i>Ribes alpinum</i> Alpine Currant	T	—	T
<i>Ribes nigrum</i> Black Currant	T	—	T, M
* <i>Robinia pseudoacacia</i> Black Locust (native invasive, not recommended)	T	T	T
<i>Rosa canina</i> Dog Brier Rose	S	—	S
<i>Rosa rugosa</i> Rugosa Rose	T, S	—	T
<i>Rosa virginiana</i> Virginian Rose	S	—	—
<i>Salix alba</i> White Willow	T, M, S	—	M
<i>Salix alba</i> 'Tristis' Golden Weeping Willow	S	T	T
<i>Salix alba</i> 'Vitellina' Golden Willow	M, S	T, M	T, M
<i>Salix caprea</i> Goat Willow	M	—	M
<i>Salix fragilis</i> Crack Willow	T	T	T
<i>Salix nigra</i> Black Willow	M	—	—
<i>Salix pentandra</i> Laurel Willow	M	—	M
<i>Salix purpurea</i> Purple Osier Willow	—	T, M	T
<i>Salix purpurea</i> 'Nana' Dwarf Arctic Willow	—	S	S
<i>Sambucus</i> species Elderberry	S	—	—
<i>Shepherdia argentea</i> Buffalo Berry	T, M	—	T
<i>Sophora japonica</i> Japanese Pagoda-tree	S	—	—

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Table 2. Continued

Plant Name	Salt Spray	Soil-Borne Salt	Other Salt Source
<i>Sorbus aucuparia</i> European Mountain-ash	M, S	—	S
<i>Spiraea x bumalda</i> Bumalda Spirea	S	—	M
<i>Spiraea x vanhouttei</i> Vanhoutte Spirea	—	S	T
<i>Symphoricarpos albus</i> Snowberry	T, M	—	T
<i>Symphoricarpos orbiculatus</i> Coralberry	S	—	S
<i>Syringa reticulata</i> ( <i>Syringa amurensis</i> var. <i>japonica</i> ) Japanese Tree Lilac	M	—	—
<i>Syringa vulgaris</i> Common Lilac	M, S	—	M
<i>Tamarix</i> species Tamarisk	T	T	T
<i>Taxus baccata</i> English Yew	S	—	—
<i>Taxus cuspidata</i> Japanese Yew	M, S	—	T
<i>Thuja occidentalis</i> American Arborvitae	M, S	M	T, M
<i>Tilia americana</i> American Linden, Basswood	M	S	S
<i>Tilia cordata</i> Littleleaf Linden	T, S	T, M, S	M, S
<i>Tilia x euchlora</i> Crimean Linden	S	—	—
<i>Tilia platyphyllos</i> Largeleaved Linden	T	—	—
<i>Tsuga canadensis</i> Canada or Eastern Hemlock	S	S	S
<i>Ulmus americana</i> American Elm	M	T, M, S	T, M, S
<i>Ulmus carpinifolia</i> Smoothleaf Elm	M, S	T	—
<i>Ulmus glabra</i> Scotch Elm	T	M	T
<i>Viburnum</i> species Viburnum	S	—	S

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T = tolerant    M = intermediate    S = sensitive    — = No information available    \* = invasive, not recommended in Indiana

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## Johanna Schwarz

---

**From:** David Griffiths <deglakewood@ameritech.net>  
**Sent:** Wednesday, October 2, 2024 6:41 PM  
**To:** Planning Dept  
**Subject:** Docket 1-88-24 Irene and Donald Ave.  
**Attachments:** Exhibit 1.jpg; Exhibit 2.jpg; Exhibit 3.jpg; Exhibit 4.jpg; Exhibit 5.jpg; Exhibit 6.txt

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

From: David Griffiths  
1311 Donald Ave

To: Lakewood Planning

Hello, Some concerns -

1) The County still has it owned by the Railroad. Since 1975. I would hope you get the agreement between the RR and the proposal entities. You would assume that have a Right of First refusal or other supporting documents.

2) Exhibit 1,2,3. Our privacy will be gone. When did they build housing in Lakewood where 11) 40' ft. high homes have a 'birds eye view' of all the rear yards on about 8 existing homes on Donald and Thoreau Ave's. Usually, builds have staggered windows and accommodating layouts.

3) Exhibit 4,5,6. Donald Ave is in need of repaving. All of it. The road bed is exposed and being filled with blacktop patches. Every year, more chunks of concrete and more patches. Nothing is currently being planned. Nothing has been done since 1996. 28 years ago. It won't last. I cut the grass from time to time on the road in the road in front of my house and towards the north.

Exhibit 4. This survey can't be a predictor of human behavior. It looks a little shy on the counts to me.

Thanks!

David Griffiths



ISSUE	DATE	DESCRIPTION
1	05.20.24	schematic design
2	07.10.24	sdr & planning review
3	08.07.24	sdr revisions
4	09.10.24	sdr & planning review

Light of Sight



**BEEGAN**  
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project no. 23-075

**Hilane Realty  
Donald & Thoreau  
Development**

ppn 312-26-102  
lakewood, oh 44107

Architectural Site Plan

# A1

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**VIEW AT WEST END**  
NFB

Line of sight.

ISSUE	DATE	DESCRIPTION
1	05.20.24	schematic design
2	07.15.24	site & planning review
3	08.07.24	site revisions
4	09.15.24	site & planning review



LEFT SIDE ELEVATION  
1/11/24



FRONT ELEVATION  
1/11/24



REAR ELEVATION  
1/11/24

Rear Elavation.



RIGHT SIDE ELEVATION  
1/11/24



PAUL P. BEGAN LICENSE #10274  
EXPIRATION DATE: 12.31.2025

# BEEGAN

ARCHITECTURAL DESIGN

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project no. 23-075

Hilane Realty  
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Development

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Tolwood, OH 44107

Townhouses  
Exterior Elevations

# A7

### PROPOSED TRIP GENERATION CALCULATIONS

Trip generation calculations were performed utilizing data contained in the **Trip Generation Manual, 11<sup>TH</sup> Edition** and the methods outlined in the (ITE) **Trip Generation Handbook**. Based on the previously discussed trip generation analysis procedures, the table below shows the estimated site generated traffic during the AM and PM peak hours of the adjacent streets for the proposed residential development. A copy of the trip generation worksheets can be seen in **Appendix A**.

#### NEW TRIP GENERATION

ITE TRIP GENERATION			TRIP ENDS			
ITE CODE	DESCRIPTION	Homes	Weekday AM Peak Hour of Adjacent Streets (Enter/Exit)		Weekday PM Peak Hour of Adjacent Streets (Enter/Exit)	
210	Single-Family Detached Housing	1	0	1	1	0
220	Multifamily Housing (Low-Rise)	10	6	20	16	9
TOTAL NEW TRIPS			27		26	

Who didn't come home?

### CONCLUSIONS

The previous table shows that the proposed residential development is expected to generate a total of 27 trips in the AM peak hour and 26 trips in the PM peak hour. It is our opinion that, when the anticipated changes in traffic volumes are at these levels, the traffic generated by the development should not have an impact on the surrounding street network system.

This opinion is based upon the fact that traffic impact studies are recommended to be performed by the **Institute of Transportation Engineers** whenever an increase in trips in any peak hour is greater than 100 trips per hour. This recommendation is made because this is the point where a change in roadway capacity may be found and mitigation may or may not be needed. The anticipated generated volumes from this development are less than daily variations in the current volumes on the local roadway network and should not be perceived by the traveling public.



Cindy A. Marx

From:  
cindy.marx@lakewoodoh.gov  
To:  
David Griffiths

Sat, Feb 10 at 11:42 AM

Hi David,  
I am sure the email below is not what you wanted to hear. The City has a pretty rigorous street repair schedule that they are pretty good at adhering to. Of course, if road conditions are very problematic notify Chris Gordon our Director of Public Works. He is very responsive.

Please let me know if there is anything I can do to assist. I, myself, will drive down Donald to check out road conditions.

Hope you have a wonderful weekend!

Cindy Marx  
Lakewood City Council | Ward 4  
Council Office - 216-529-6055  
Cell - 216-534-1772  
<https://www.lakewoodoh.gov/report-a-problem-concern/>

Sent from mobile

Begin forwarded message:

From: Chris Gordon <Chris.Gordon@lakewoodoh.net>  
Date: February 9, 2024 at 3:44:13 PM EST  
To: "Cindy A. Marx" <Cindy.Marx@lakewoodoh.gov>  
Subject: RE: Donald Ave.

Hi Cindy,

Donald is not on our list for resurfacing for this upcoming year. Being that the sewer line was replaced in 1996 and the street was resurfaced the same year withing that project I can add this to our list of streets to look into for future resurfacing.  
Any questions or concerns please let me know.

Thanks,

Chris Gordon

Public Works Director

City of Lakewood, Ohio

12650 Detroit Avenue

Lakewood, Ohio 44107

Phone: 216-529-6812

Email: [Chris.Gordon@lakewoodoh.net](mailto:Chris.Gordon@lakewoodoh.net)

Website: [www.lakewoodoh.gov](http://www.lakewoodoh.gov)

From: Cindy A. Marx <[Cindy.Marx@lakewoodoh.gov](mailto:Cindy.Marx@lakewoodoh.gov)>

Sent: Friday, February 9, 2024 3:27 PM

To: Chris Gordon <[Chris.Gordon@lakewoodoh.net](mailto:Chris.Gordon@lakewoodoh.net)>

Subject: Fwd: Donald Ave.

Hi Chris,

Is Donald due to be resurfaced this year?

Thanks.

Cindy Marx

Lakewood City Council | Ward 4

Council Office - 216-529-6055

Cell - 216-534-1772

<https://www.lakewoodoh.gov/report-a-problem-concern/>

Sent from mobile

Begin forwarded message:

From: David Griffiths <deglakewood@ameritech.net>  
Date: February 9, 2024 at 1:19:40 PM EST  
To: "Cindy A. Marx" <Cindy.Marx@lakewoodoh.gov>  
Subject: Donald Ave.  
Reply-To: David Griffiths <deglakewood@ameritech.net>

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Hello Cindy,

I was wondering if there are resurfacing planned for Donald Ave? It has become atrocious since they replaced the sewer line years ago. It needs resurfacing.

Thanks.

David Griffiths 1311 Donald Ave.

216-255-8198

Sent from AT&T Yahoo Mail on Android

## Johanna Schwarz

---

**From:** Donny Davis <donny.davis4@gmail.com>  
**Sent:** Wednesday, October 2, 2024 7:15 PM  
**To:** Planning Dept  
**Subject:** Docket No. 10-37-24 - Planning Commission Submission  
**Attachments:** Docket No. 10-37-24 - Davis - 1280 Thoreau - Planning\_Commission Submission\_10.3.24.pdf

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Please find the attached comments for submission regarding Docket No. 10-37-24 at the 10/3/24 Planning Commission meeting.

Thank you,  
Donny & Jackie Davis  
1280 Thoreau Road  
Lakewood, OH 44107  
330-807-3046

Donny & Jackie Davis  
1280 Thoreau Road  
Lakewood, OH 44107

October 2, 2024

City of Lakewood Planning Commission  
12650 Detroit Avenue  
Lakewood, OH 44107

Re: Docket No: 10-37-24

Members of the Planning Commission:

We are residents at 1280 Thoreau. Our property is contiguous with adjacent yard owned by Mr. Kermode and within viewshed of the site from various angles. That said, we are writing to express our opposition regarding the proposed townhome development at Donald/Thoreau on the Norfolk Southern property. Though there are many, here are the key concerns below:

The proposed re-zoning of the R-2 parcel to PD creates significantly more density, traffic, light and noise pollution, and introduces housing product that is not characteristic of the surrounding neighborhood. Additionally, with the proposed heights, the housing towers over the surrounding and insular part of the neighborhood - versus being on a main arterial like Detroit or Clifton. The rear balconies will also infringe upon the privacy that neighbors currently enjoy. The consensus among neighbors is that detached single-family residential housing could exist under current R-2, however the sheer density of this re-zone and proposal has too many ancillary negative impacts.

Rezoning a parcel of this size should be tabled until the 2024 Lakewood Zoning Refresh is complete, as the outcomes and recommendations could have impacts on current proposal based on any new guidelines/processes.

Additionally, despite attending meetings with applicant and ARB, there have been no design or orientation concessions/alterations to proposed plan throughout the process. We understand the Planning Commission/ARB does appreciate the trail introduced for connectivity to Cove Park from Donald to Thoreau, however this is only one small section of the railroad tracks that runs through the entire city and could be achieved without the development along with longer-term mobility visioning.

Finally, we would like for City Council/Planning Commission to help residents understand more about the out-of-state developer, Hilane Realty, and their credentials/ability to build the proposed product that is of lasting quality. Observationally, the company's website ([www.hilanerealty.com](http://www.hilanerealty.com)) is generic and shows no previous development portfolio work to indicate its proven track record. Instead, the company website shows only that it owns one multi-family property in Shaker Heights.

Thank you for your consideration.

Donny & Jackie Davis  
1280 Thoreau Road  
330-807-3046

## Johanna Schwarz

---

**From:** katie gallagher <katieann520@yahoo.com>  
**Sent:** Thursday, October 3, 2024 10:50 AM  
**To:** Planning Dept  
**Subject:** Thoreau Townhomes

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Dear Planning Committee,

I am writing to state my opposition to the Thoreau Townhomes project. I have lived at 1288 Thoreau for almost 12 years and will directly affected by the proposed project. Many of my neighbors feel the same way. Please at least add this topic to the meeting today so that it can be discussed further.

Thank you for your time and consideration.

Sincerely,

Katie Gallagher

## Johanna Schwarz

---

**From:** Anna Harrington Gartland <akh7281@gmail.com>  
**Sent:** Thursday, October 3, 2024 12:02 PM  
**To:** Planning Dept  
**Subject:** Donald Thoreau Development Comments

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Good morning,

We are homeowners and residents of 12593 Clifton Boulevard, on the corner of Thoreau Road. We were saddened to learn of the proposed Donald Thoreau development by some of our neighbors on Thoreau. We moved to Lakewood from the Boston area 5 years ago and have grown to love the community for its tight-knit neighborhoods and respect to nature and historic architecture.

The Donald Thoreau project would remove a good deal of the precious little open green space from the neighborhood, along with old growth trees, which both serve as habitat for Lakewood's diverse wildlife. It would also increase population density in an already very dense area where parking and navigation is often challenging. As parents, we also have grave concerns about the safety implications, given that children frequently ride bikes, play in Cove park and cross the street near the railroad tracks.

Please consider carefully the negative impacts this project would have on the neighborhood and Lakewood community as a whole.

Thank you for your attention,  
Anna & Kevin Gartland  
617-417-9951

## Johanna Schwarz

---

**From:** O'Shea, Kitty <Kioshe@ccf.org>  
**Sent:** Thursday, October 3, 2024 3:06 PM  
**To:** Planning Dept  
**Subject:** Oct 3 4PM Meeting Docket No. 10-98-24 Donald Thoreau Development Project

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello, Here are my questions/comments for Docket No. 10-98-24 Donald Thoreau Development Project. I'm going to attempt to join the Zoom meeting.

Why weren't all Donald residents notified?

We will lose parking spaces at the end of Donald.

Who can park in the 6 spaces on the design?

Traffic study is not realistic. Example, a single home is going to have only one car leaving and one car returning during peak hours?

People come and go all day long.

Irene appears to be more of a driveway than a street.

How wide is the entrance off Donald? Is it as wide as an actual street?

We already have cars wiggling around the intersection of Merl and Donald.

What about snow removal? End of Donald used to pile snow.

If the neighborhood is not supportive of this, who benefits?

We want to be good neighbors, help us to maintain that goal.

If developers want to build in Lakewood, allow them fill the empty hospital site.

Thank you,

Kitty O'Shea  
1296 Donald Ave  
216 310 8169

Please consider the environment before printing this e-mail

Cleveland Clinic is a nonprofit, multispecialty academic medical center that's recognized in the U.S. and throughout the world for its expertise and care. Visit us online at <http://www.clevelandclinic.org> for a complete listing of our services, staff and locations. Confidentiality Note: This message is intended for use only by the individual or entity to which it is addressed and may contain information that is privileged, confidential, and exempt from disclosure under applicable law. If the reader of this message is not the intended recipient or the employee or agent responsible for delivering the message to the intended recipient, you are hereby notified that any dissemination, distribution or copying of this communication is strictly prohibited. If you have received this communication in error, please contact the sender immediately and destroy the material in its entirety, whether electronic or hard copy. Thank you.

## Johanna Schwarz

---

**From:** Debbie Marshall <debommarshall@gmail.com>  
**Sent:** Thursday, October 3, 2024 4:22 PM  
**To:** Planning Dept  
**Subject:** Planning Committee Meeting 10-37-24

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello,

My name is Debbie Marshall and as a long-time resident (8 years in December) of 1281 Thoreau Rd, I am very concerned about the ramifications of the proposed rezoning and development of the lot between Thoreau and Donald, especially in light of the newest version of the plans that took into account none of the numerous letters, emails, calls, and conversations from my neighbors expressing our concerns.

Being the only thru street between Nicholson and Cove (and one often used by Police, Fire, and EMT), Thoreau Rd is already congested in terms of traffic. With numerous residents and the entrance to Cove Park across from the lot, parking is already scarce. Adding 10 units would make this neighborhood untenable in terms of navigation and habitation. Prior to the finished townhomes, the development and construction leading up to the finished townhomes will only add to this congestion and the frustrations and inconvenience of the existing neighborhood residents who previously chose to reside on and invest in Thoreau without these complications.

In addition to the safety concerns posed by traffic, this many units would add a considerable amount to the population of Thoreau Rd which is another safety concern and drain on the resources of the neighborhood.

I also question the viability of multiple expensive townhomes built feet from an active railroad track and a school athletic field where many loud, well attended public events take place on a regular basis, causing light and sound pollution in the neighborhood. This reeks of an attempted money grab from the owners of the plot of land, Norfolk Southern, who has already done enough damage to the land and residents of Ohio and I would hate to see the City of Lakewood complicit in this type of vile venture.

The residents of Thoreau are not unreasonable to change and 'progress' for our city so If development is to move forward, please consider allowing single-family homes, not townhomes. Also the private drive could run along the railroad tracks to further distance the new inhabitants from the railroad tracks and allow more privacy for the existing neighbors whose property is parallel to proposed development.

Thank you for your time and consideration.

Sincerely,  
Debbie Marshall  
1281 Thoreau Rd Resident



## Johanna Schwarz

---

**From:** Cindy A. Marx  
**Sent:** Saturday, October 5, 2024 12:45 PM  
**To:** Katherine Herten; David Baas  
**Cc:** Amanda L. Cramer; Planning Dept  
**Subject:** Re: Proposed development on Thoreau

Again, Katie, thank you for your thoughtful message. I did give Heidi's message to David Baas for entry into the meeting record. I do hope the planning commission reconsiders the appropriateness of this development into the neighborhood.

Enjoy your weekend.

Cindy Marx (she, her)  
Lakewood City Council  
Ward 4 Representative  
Cell: 216-534-1772  
Cindy.marx@lakewoodoh.gov

<https://www.lakewoodoh.gov/>  
<https://www.lakewoodoh.gov/report-a-problem-concern/>

---

**From:** Katherine Herten <kbunsey@gmail.com>  
**Sent:** Saturday, October 5, 2024 12:20:55 PM  
**To:** David Baas <David.Baas@lakewoodoh.gov>  
**Cc:** Amanda L. Cramer <Amanda.Cramer@lakewoodoh.gov>; Planning Dept <Planning@lakewoodoh.net>; Cindy A. Marx <cindy.marx@lakewoodoh.gov>  
**Subject:** Re: Proposed development on Thoreau

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello Planning Department, Ms. Marx, Ms. Cramer and Mr. Bass.

Thank you for your response, as well as the few extra minutes to speak on behalf of Thoreau residents on the south end of our street; residents who are not even aware of the project proposal. Again, I believe the development proposal creates an equity issue for residents who do not have their own driveways or yards; folks who lack access to their own green spaces, parking spaces and trees on their properties. The south end of Thoreau is such a narrow street that we didn't qualify for tree lawn trees provided by the city's canopy program. Thankfully, I could have a tree placed in my front yard. Other residents on Thoreau are renters and since their property owners may have not received the letters from the city or perhaps they disregarded the letters about free trees ~ my neighbors did not benefit from this wonderful

program. I mention the tree program because I think it lends itself to the discussion of the development proposal.

I heard talk during Thursday's meeting about planning for the future, maintaining green spaces and increasing our tree canopy. These are topics of tremendous importance to me as a steward of the Lake Erie watershed and ecosystem. Cleveland has been my home for 47 years and over the past 4+ decades I have witnessed this region go through a tremendous revitalization of our urban spaces, repurposing of industrial areas, and a renewal of care for our natural resources, with an emphasis on connecting our city neighborhoods to nature. These are the things that give me hope that maybe we, too, can do something in our own community to help.

This morning I saw a Facebook post about trees and storm water run off. I mentioned carbon sequestration during the meeting and I heard the developer mention storm water run off during their presentation. Our storms are getting increasingly more severe. Just this past week, North Carolina received catastrophic levels of rain and flood damage. We have problems on this planet and in our own region because of climate change. Residents in Lakewood have had sewer backups and flooding like never before due to the volume of precipitation that comes with our new storm systems and an overburdened/outdated sewer system. Our growing zone and climate here in Ohio has recently been changed to that of NC or West Virginia. We have a crisis on our hands with a changing planet and can do something about it with simple acts of trees and gardens, deep root systems and less pavement.

What if we in Lakewood were bold and dared to be different than our western neighbors such as Westlake, Avon and North Ridgeville ~ communities who are plowing under every remaining wood lot and farm field to squeeze in a shopping mall or housing development. Those communities are not developing walking and biking networks for their children and residents to get to schools, shops and restaurants as we are. What if we did something radical and said, No Thank You and turned one of our few remaining green spaces into a Homegrown National Park, or a flower forest for birds, monarchs and other pollinators or created a space that could become an urban outdoor learning opportunity for residents, children and guests to our community. What if we served as a model for the type of change that truly embodies, Thinking About Tomorrow and seeks to leave a better path for our children.

In Lakewood, we speak of diversity, equity, belonging and citizenship ~ we have a school that has developed a partnership with an urban farm that provides opportunities for refugees, as well as delicious, local produce for greater Cleveland. The children of Harrison Elementary will benefit for a lifetime from something as simple as growing their own food at school. The City of Lakewood could follow in the path forged by teachers and children at Harrison and build a better tomorrow simply by daring to do something different with our green spaces. Rather than developing this space, we could choose creativity and regeneration.

Finally, there was a resident who lives on Donald at the meeting who had to depart earlier than planned. She left me with a printed copy of her thoughts and asked if I could pass them along. I gave the sheet to our council person, Cindy Marx and am attaching it here, along with the tree message I read on Facebook this morning.

Thank you, again, for considering my request and taking the concerns of residents seriously.

Katie Herten  
1347 Thoreau

P.S. An adult bald eagle flew over the homecoming game at Lakewood High School last evening. The crowd went wild! When I was a child, we didn't have bald eagles in Cleveland. This eagle sighting was made possible first by an astute observer of nature, Rachel Carson, then the Burning River scenario in Cleveland that prompted the Environmental Movement and the hard work of concerned citizens, government leaders and industry. Let's enjoy more bald eagle sightings ~ more screech owls hoots at night ~ more monarchs and more connection with nature here in our backyards! These nature connections are vital to our wellbeing.

11:11

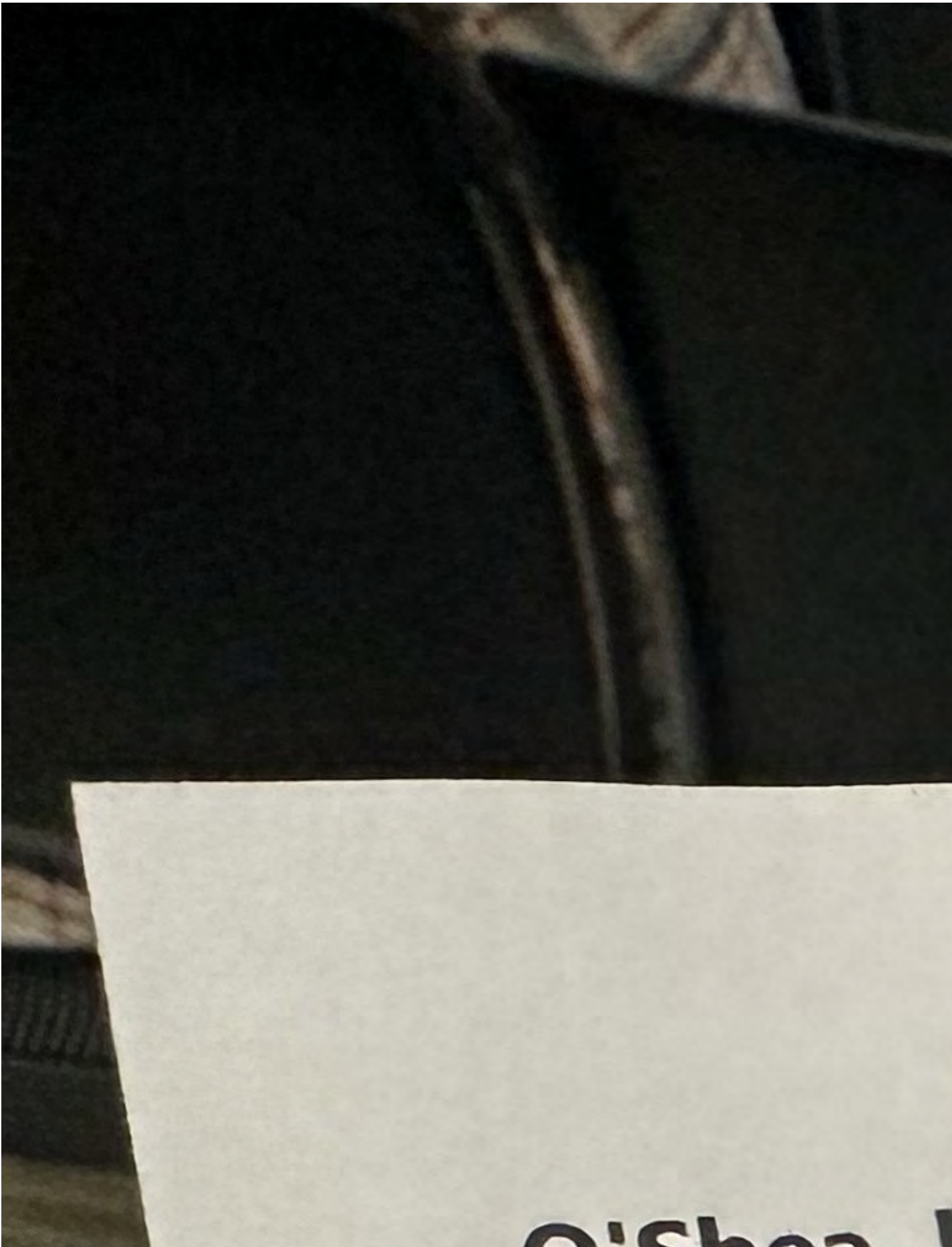


**Cuyahoga Soil & Water**

1h · 

**Happy Saturday!**

**Tell us the answer to this question in the comments and win native seeds. (first person only)**



On Thu, Oct 3, 2024 at 2:09 PM David Baas <[David.Baas@lakewoodoh.gov](mailto:David.Baas@lakewoodoh.gov)> wrote:

Katie,

To reinforce Amanda's response – yes, please attend the meeting tonight and speak with the Commission during the Public Comment portion for this item.

Thank you –

Sincerely,

Dave

**Dave Baas, AICP**

Assistant Director, Planning & Development

LtCol, USMC (Retired)

[City of Lakewood](#)

[12650 Detroit Avenue](#)

Lakewood, Ohio 44107

[david.baas@lakewoodoh.gov](mailto:david.baas@lakewoodoh.gov)

(216) 529-6637 (work)

(216) 372-8996 (cell)

[www.lakewoodoh.gov](http://www.lakewoodoh.gov)

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**From:** Katherine Herten <[kbunsey@gmail.com](mailto:kbunsey@gmail.com)>

---

**Sent:** Monday, September 30, 2024 3:29 PM

**To:** David Baas <[David.Baas@lakewoodoh.gov](mailto:David.Baas@lakewoodoh.gov)>

**Cc:** Amanda L. Cramer <[Amanda.Cramer@lakewoodoh.gov](mailto:Amanda.Cramer@lakewoodoh.gov)>; Planning Dept <[Planning@lakewoodoh.net](mailto:Planning@lakewoodoh.net)>

**Subject:** Re: Proposed development on Thoreau

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Thank you, Mr. Baas. If I would like to speak at the meeting on Thursday, does this note suffice as notice or should I officially register somewhere?

Katie

On Mon, Sep 30, 2024 at 2:54 PM David Baas <[David.Baas@lakewoodoh.gov](mailto:David.Baas@lakewoodoh.gov)> wrote:

Ms. Herten,

Thank you for the email – it will be forwarded to the Planning Commission for their consideration and included with the record for this item.

Sincerely,

Dave Baas

**Dave Baas, AICP**

Assistant Director, Planning & Development

LtCol, USMC (Retired)

[City of Lakewood](#)

[12650 Detroit Avenue](#)

Lakewood, Ohio 44107

[david.baas@lakewoodoh.gov](mailto:david.baas@lakewoodoh.gov)

(216) 529-6637 (work)

(216) 372-8996 (cell)

[www.lakewoodoh.gov](http://www.lakewoodoh.gov)

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**From:** Katherine Herten <[kbunsey@gmail.com](mailto:kbunsey@gmail.com)>

**Sent:** Friday, September 27, 2024 11:56 PM

**To:** Planning Dept <[planning@lakewoodoh.gov](mailto:planning@lakewoodoh.gov)>

**Subject:** Proposed development on Thoreau

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Dear Lakewood Planning:

I am a resident and homeowner at [1347 Thoreau Road](#). I am also a Lakewood City Schools teacher of over 20 years. I am proud to teach, live and send my children to our schools and participate as a member in this community.

I was very disappointed to learn of the proposal to build a road, a home and a building with condos in the green space just north of the train tracks on Thoreau, across from Cove Park.

This development project threatens the safety of residents, especially children who live on Thoreau, as well as guests to Cove Park. Thoreau is already a densely populated street with limited parking for the residents in the duplex units on the south end of the street, many of whom do not even have driveways. Our street receives heavy traffic from safety and security vehicles, city vehicles and serves as a cut through for other drivers. Adding an access road to a multi resident complex near the almost 90 degree bends in Thoreau north of the tracks, poses increased safety risks as this portion of the street is already challenging for north and southbound traffic to navigate. Adding a road near this section of the street increases the potential for traffic accidents. Visibility is an issue when navigating the bends by vehicle. Usually cars slow down or pull to the side at this part of our street to allow each other to safely pass. Oftentimes on the southern portion of Thoreau, oncoming cars must pull over to wait for traffic moving in the opposite direction because of the reduced road space with completely filled street parking. Additionally, events at Cove Park such as children's recreation ball games increase the traffic and parking demands on Thoreau since the Cove parking lot cannot handle the quantity of cars for these events.

Aside from human density and driving safety issues, it is extremely disappointing that the City of Lakewood would consider developing one of our few remaining open green spaces. The addition of a multi-unit condo complex will increase light pollution for residents, which is harmful to our health. Light pollution has detrimental effects for migratory organisms as well. There are several very large/old trees near the tracks which provide habitat for owls, hawks and other birds. The area planned for development is nice clearing of grass that could be otherwise turned into vital prairie habitat for monarch butterflies, who are now declared endangered, as well as other organisms who are part of our Lake Erie ecosystem.

I beg you, Lakewood, to reconsider this plan to develop Thoreau Road for issues of safety and population/vehicle density. It is my belief that part of Keeping Lakewood Beautiful includes saving our remaining green spaces and minimizing the burden on areas that are already challenged by population density.

Thank you,

Katherine Herten

216-402-7496

## Johanna Schwarz

---

**From:** Brian Meng <bmeng@bialosky.com>  
**Sent:** Thursday, November 7, 2024 12:16 PM  
**To:** Johanna Schwarz; Amanda L. Cramer; David Baas  
**Cc:** Ian Andrews; Amy Haney; Adam Branscomb  
**Subject:** RE: Agenda: Planning Commission - 11/7/2024

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Johanna, Amanda, and Dave,

On behalf of LakewoodAlive we wanted to provide comments and observations to the projects on the November 7, 2024, PC meeting agenda.

- **Docket 10-38-24: Jersey Mike's Subs conditional use**
  - The proposed outdoor seating is a nice way to provide activation and support the conditional use.
  - We wonder if there are opportunities to add planters to the patio or against the adjacent walkway to add to the visual interest.
- **Docket 07-30-24 & 07-31-24: Complete Streets Initiatives**
  - We are excited to see this resolution on the Agenda and equally excited to see its continued passage through the appropriate boards, commissions, committees, etc., and ultimately in implementation. We support this ordinance.
- **Docket 10-37-24: Irene & Donald Residential Development**
  - We appreciate the sidewalk connections to Donald and Thoreau avenues, the uniqueness of the site, and the proposed developments plan for the vacant parcel.
  - No additional comments to the updates made from the August ARB meeting submittal.
- **Docket 11-39-24: 1517 Alameda Ave.**
  - We support the reduction of parking minimum for this plan.
- **Docket 11-40-24: ODOT Excess Land Transfer @ Glenbury.**
  - We are curious if, after transfer to the City, this small portion of Glenbury will remain roadway or if the City will remove the pavement and consolidate it with the open space on either side.
- **Docket 11-41-24: ODOT Excess Land Transfer @ Niagara.**
  - After transfer are there any improvements planned by the city for this parcel, such as aligning the curbs on Marginal where the former roadway existed?
- **Docket 11-42-24: Lake-Clifton Connector Project**
  - We are intrigued by the description of this proposal as an opportunity to enhance the connection between Rocky River and Lakewood.
  - We look forward to the opportunity to review any available graphic plans and diagrams illustrating the intent.
  - We are curious of what improvements are proposed for the bridge and how they would be implemented.

- We are also curious if there is enough existing ROW for the proposed roundabout at the Lake Road and Clifton Boulevard intersection.
- **2024 Beautiful Home Awards**
  - Great to see so many homes being recognized.

We truly appreciate the opportunity to share our thoughts with you on these applications. If you have any questions on our comments, please let us know and we would be happy to discuss further. It is exciting to see so many developments occurring in Lakewood.

Thank you,

**BRIAN MENG** RA, LEED AP  
Principal | Architect

**BIALOSKY** Always By Design.

P 216-767-2023

---

**From:** Ian Andrews <iandrews@lakewoodalive.org>  
**Sent:** Wednesday, October 30, 2024 11:08 AM  
**To:** Amy Haney <amyjhaney@gmail.com>; Brian Meng <bmeng@bialosky.com>  
**Subject:** FW: Agenda: Planning Commission

**Ian Andrews**  
Executive Director  
LakewoodAlive  
12815 Detroit Avenue  
Lakewood, OH 44107  
216-521-0655



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**Upcoming Events:**

**12/7 – 18<sup>th</sup> Annual Light Up Lakewood**



---

**From:** Johanna Schwarz <[Johanna.Schwarz@lakewoodoh.gov](mailto:Johanna.Schwarz@lakewoodoh.gov)>

**Sent:** Wednesday, October 30, 2024 11:05 AM

**To:** Johanna Schwarz <[Johanna.Schwarz@lakewoodoh.gov](mailto:Johanna.Schwarz@lakewoodoh.gov)>

**Subject:** Agenda: Planning Commission

Good morning, Everyone,

Please click on the link to view the agenda for the Thursday, November 7, 2024 PC meeting. The pre-review meeting begins at 5:30 p.m. in the auditorium, followed by the 6:00 p.m. review meeting.

[https://www.lakewoodoh.gov/wp-content/uploads/2023/12/PCAgenda\\_110724.pdf](https://www.lakewoodoh.gov/wp-content/uploads/2023/12/PCAgenda_110724.pdf)

Sincerely,

*Johanna Schwarz*

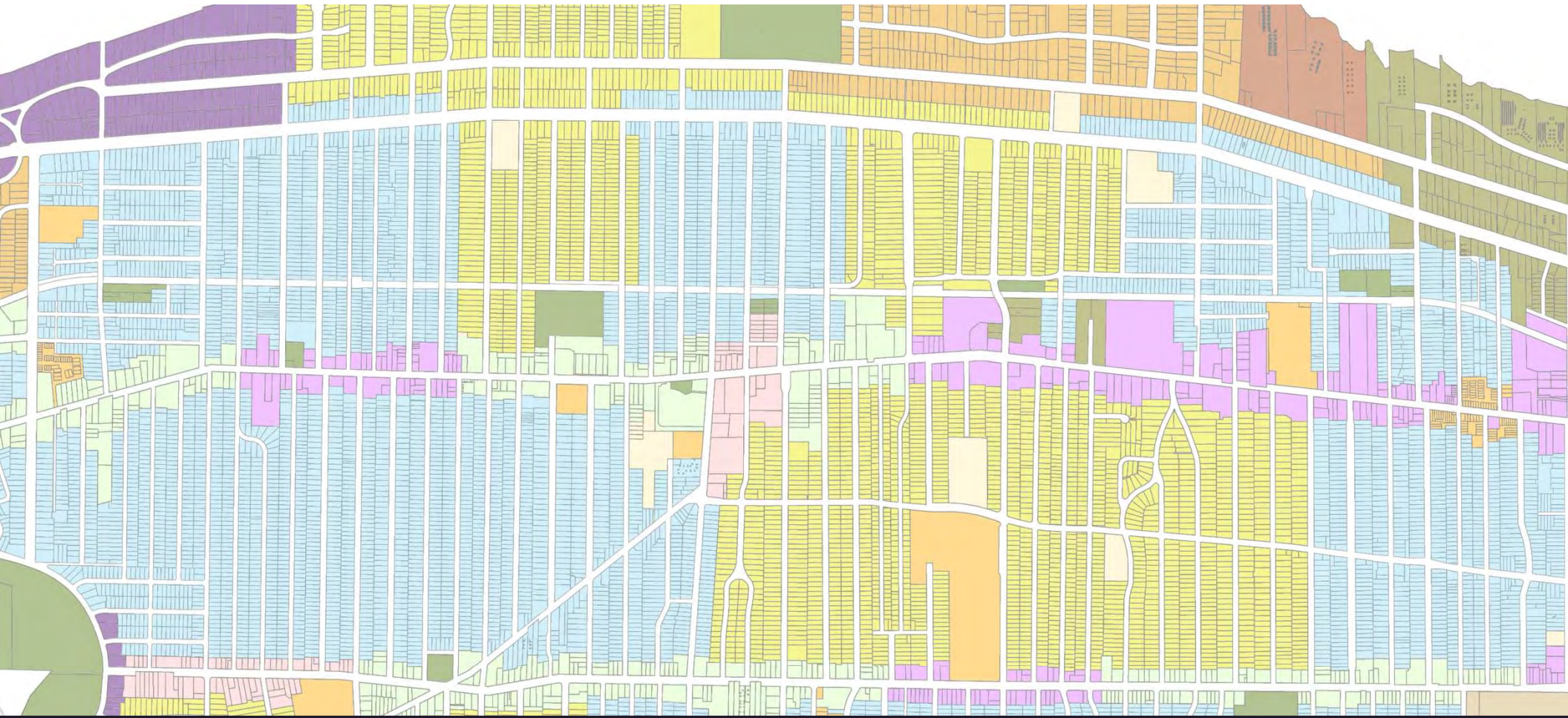
Administrative Assistant II

City of Lakewood  
Department of Planning and Development  
12650 Detroit Avenue  
Lakewood, Ohio 44107

(216) 529-6630 main  
(216) 529-6631 office

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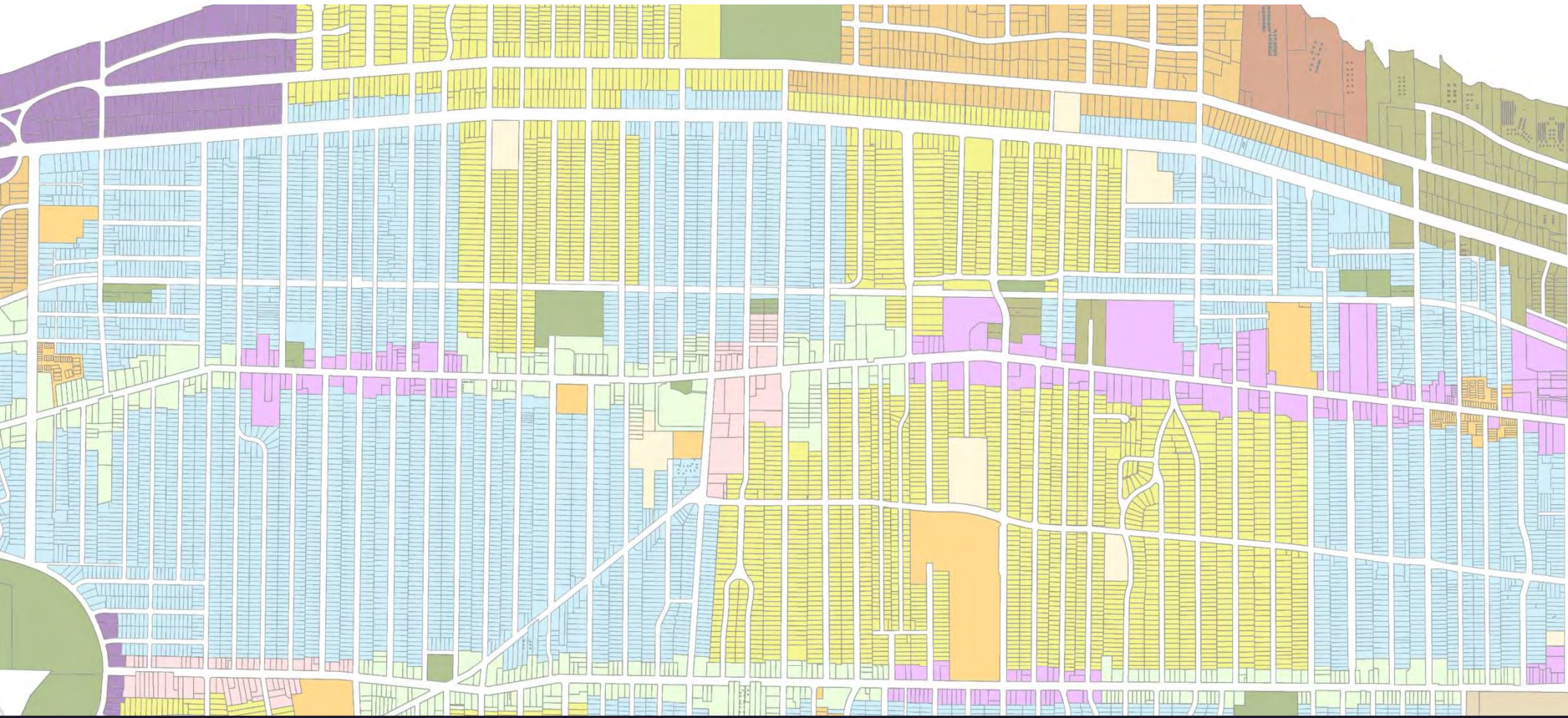
# Planning Commission

November 2024



**Planning Commission**  
**November 2024 Agenda**

1. Roll call
2. Adoption of minutes – October 2024 meeting
3. Opening Remarks
4. 11-42-24: Communication – Lake-Clifton Connector Project (Cuyahoga County)
5. 10-38-24: Conditional Use – Outdoor Dining/Jersey Mike’s)
6. 07-30-24: Complete Streets Initiative: Ordinance No. 11-2024
7. 07-31-24: Complete Streets Initiative: Resolution 2024-42
8. 10-37-24: Donald & Thoreau (Planned Development)
9. 11-39-24: Parking Plan – 1517 Alameda (R2)
10. 11-40-24: Communication – ODOT Excess Land Transfer (Glenbury – Dedication)
11. 11-41-24: Communication – ODOT Excess Land Transfer (Niagara – Vacation)
12. Adjourn



**Docket 11-42-24 - Lake-Clifton Connector Project**  
Communication for Review/Resolution 2024-68



## **Request (11-42-24)**

The review of **Resolution 2024-68 (Lake-Clifton Connector Project)**.

- City/Cuyahoga County Agreement

**Provide Recommendations, Recieve & File**



County Presentation



# Lake Road-Clifton Boulevard Corridor Improvements





16306 Detroit Ave



**Docket 10-38-24 - 16306 Detroit**  
**Jersey Mike's - Conditional Use (Outdoor Dining)**

## **Request (10-38-24)**

The review and approval of a **conditional use (outdoor dining)** pursuant to:

- Chapter 1161 – Conditional Uses.

Property is in a C3, Commercial (General) District.

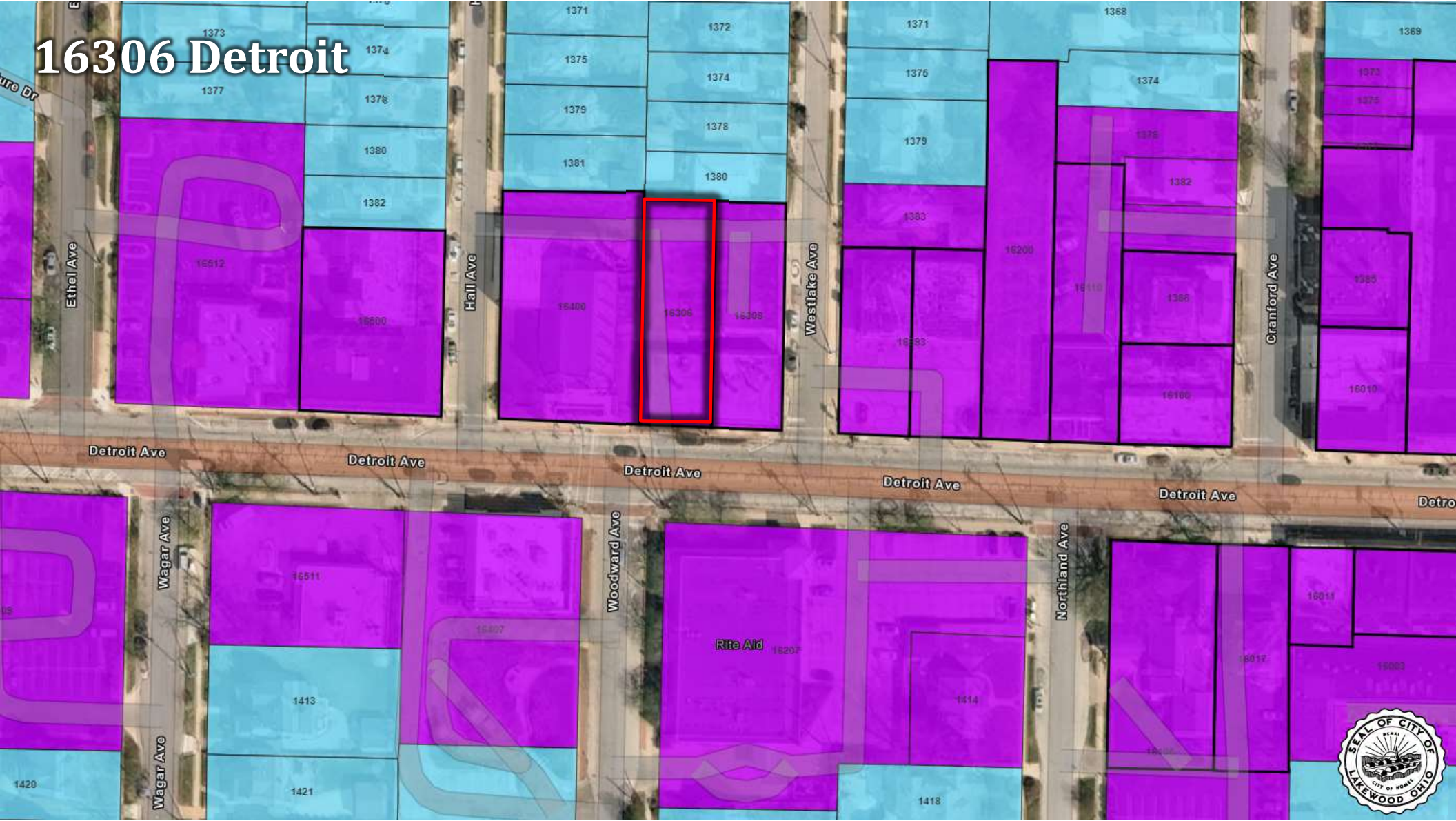


# 16306 Detroit



Near

# 16306 Detroit





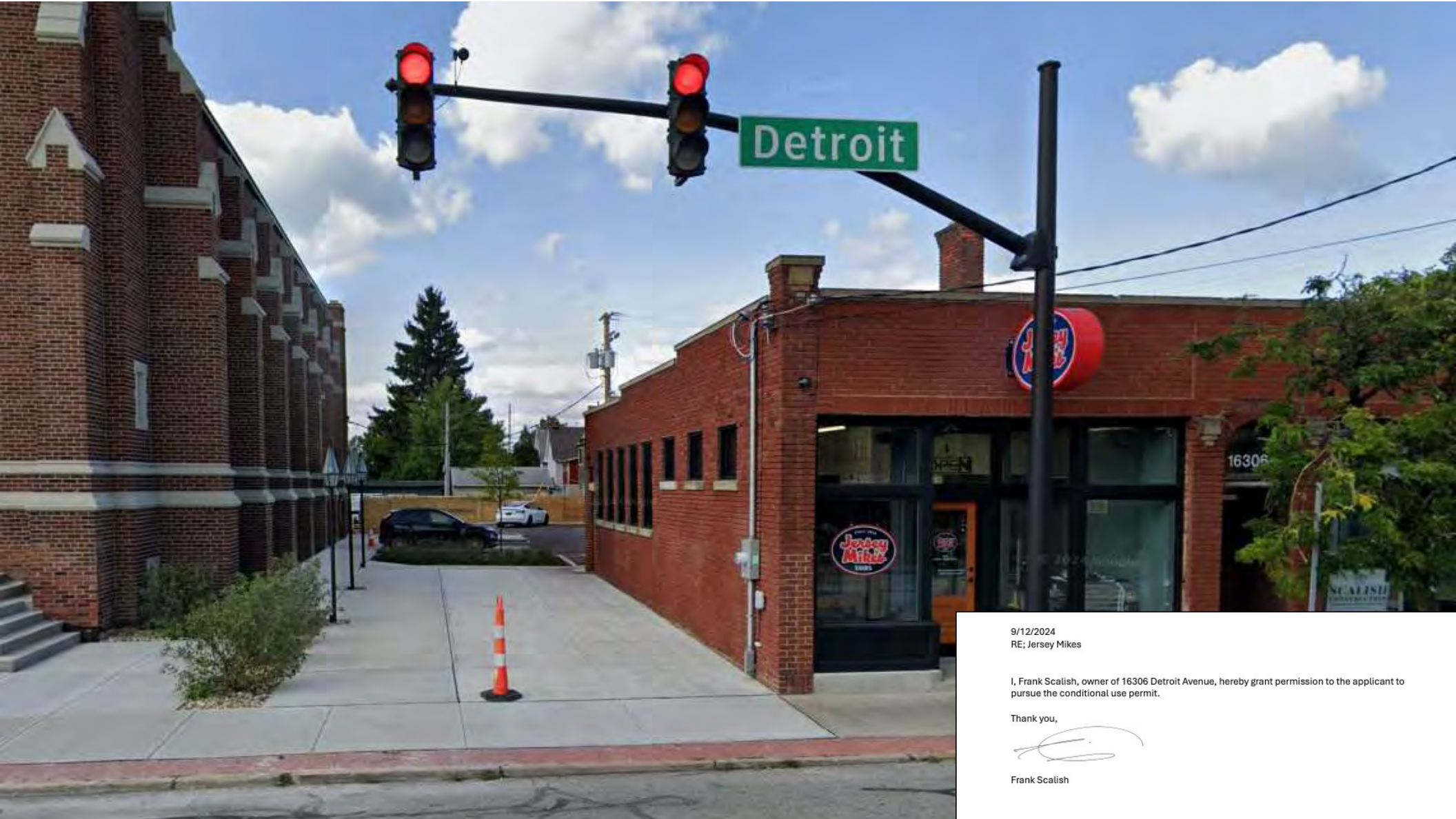
9/12/2024  
RE: Jersey Mikes

I, Frank Scalish, owner of 16306 Detroit Avenue, hereby grant permission to the applicant to pursue the conditional use permit.

Thank you,

Frank Scalish





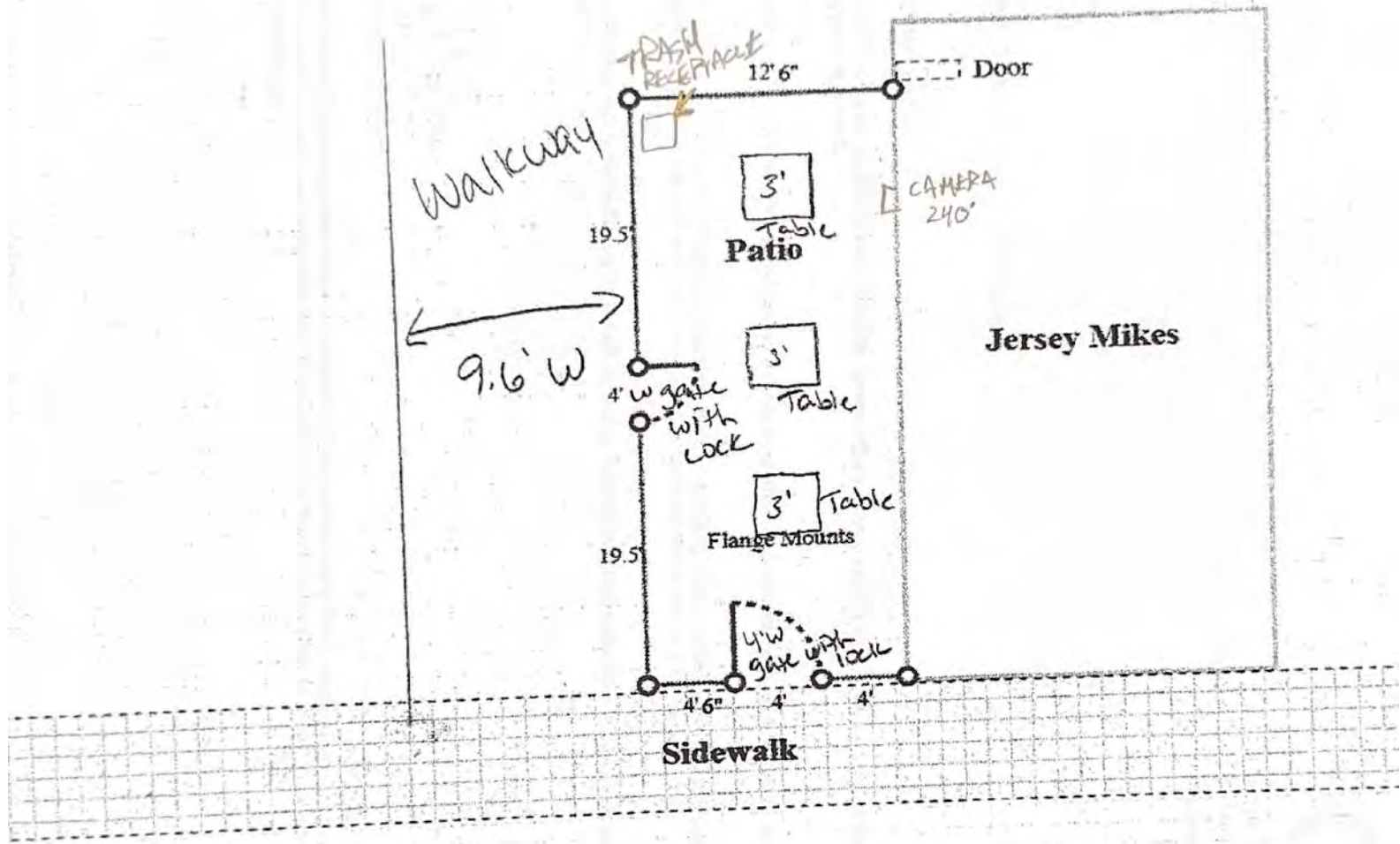
9/12/2024  
RE: Jersey Mikes

I, Frank Scalish, owner of 16306 Detroit Avenue, hereby grant permission to the applicant to pursue the conditional use permit.

Thank you,

Frank Scalish

# Parking Lot



16400



16306

16304

16310



16400



Jersey Mikes

16306

16304

16310

Sidewalk

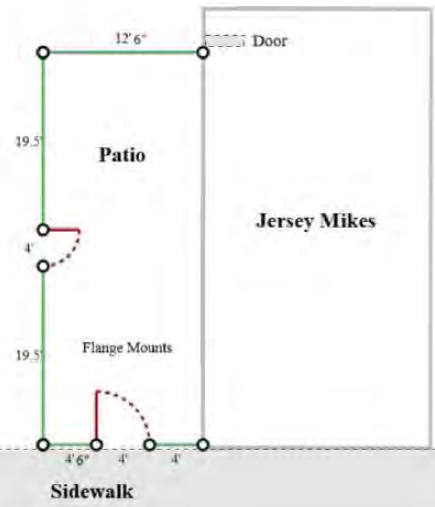


No  No

**APPROXIMATE LAYOUT**

FENCE FOOTAGE CONTAINED IN THIS PROPOSAL IS APPROXIMATE BASED ON THE FIELD MEASUREMENT, FINAL PRICE WILL BE ADJUSTED BASED ON ACTUAL FENCE FOOTAGE USED, AS SET FORTH IN TERMS AND CONDITIONS)

**FENCE DIAGRAM**



■ 4'H Black 300 Sterling (C)



Proposal for Fencing Installation



**PRODUCT SPECIFICATIONS:**

4'H Black 300 Sterling (C): 59 LF & 2 Gates.  
Specifications: Post: 2" Sq., Rail: 1-1/2"x1", Picket: 3/4" Sq.

**JOB OPTIONS:**

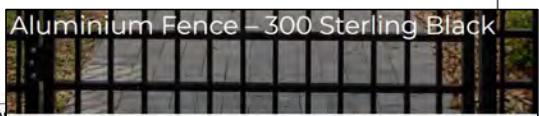
(2) 4ft SS Standard Gate, Standard Latch

**JOB NOTES:**

No Returns for Special Order Product.

Approximate Start Date:	11/15/2024	All dates at events include and manufacture
Approximate Finish Date:	11/21/2024	

Acceptance and Authorization: By signing below, you authorize Superior to perform the work that may be custom made, as specified in this Agreement. You understand and agree that all written representations or agreements made to you by Superior but, that if there are any agreements, you agree to accept the same as final. Do not sign if blank or incomplete. By signing, you acknowledge that you have read, understand and agree to the terms and conditions of your local utility.



Example pulled from website

Superior Fence & Rail Contact Information:

SW Fencing LLC  
6909 Engle Rd., Unit 35  
Middleburg Heights, OH 44130  
216-435-7096

FENCE TO FOLLOW GRADE OF PROPERTY: PLEASE INITIAL

*SN*



**Wabash Valley SU2H38P Sullivan  
42" Round Portable Powder  
Coated Steel Mesh Outdoor**



## 1161.02 GENERAL STANDARDS FOR ALL CONDITIONAL USES.

...In addition to any specific regulations required by this Code or the Ordinances, the Commission shall find:

- (a) ...will be **consistent with the general objectives, or any specific objectives, for the zoning district** in which it is located, as set forth in this Code and the Vision.
- (b) ...will be designed, constructed, operated and maintained so as **not to be detrimental to or endanger the public health, safety, or general welfare.**
- (c) **...will not change the essential character of the general vicinity** in which it is located.
- (d) **...will not be injurious to the uses permitted by right** in the immediate vicinity, nor substantially diminish or impair property values within said vicinity.
- (e) **...will not impede the normal and orderly development of uses permitted by right** in the immediate vicinity.
- (f) ...adequate utilities and public facilities and services, such as streets and sewer and water services, are or will be provided; and that establishment of the conditional use will not require expenditures of public funds for such public facilities or services.
- (g) ...adequate measures have been taken to minimize traffic congestion on public streets.
- (h) ...will not suffer substantial hardship in future due to conditional use being surrounded by uses permitted by right, which are incompatible...
- (i) ...shall, in all other respects, conform to applicable regulations of the district in which it is located as well as the specific conditions set forth in Section 1161.03.
- (j) A Conditional Use Permit issued pursuant to this Chapter may not be transferred to any other person or address.
- (k) N/A (Mixed Use Overlay development)

Notwithstanding anything in this Chapter or this Section to the contrary, where the Commissioner determines, after compliance with the requirements of Section 1173.02, that a permit holder seeking renewal of a permit issued pursuant to this Section has remained in compliance with the conditions of said permit, the Commissioner may issue such renewal.



**(t) Outdoor Dining Facility.**

Any person operating a restaurant, bar, tavern, or nightclub use...may be permitted to operate an outdoor dining facility as a conditionally permitted accessory use subject to the following:

(1) (application...)

(2) The outdoor dining facility shall be **located directly adjacent to the lawfully operating restaurant use.** The outdoor dining facility may be located immediately adjacent to the front of the restaurant use, on the side or rear of the restaurant use or as approved by the Planning Commission.

(3) The floor space of the outdoor dining facility and any walkway connecting such facility with the restaurant use and the parking lot or any public or private sidewalk shall be constructed of an approved hard surface material meeting all local, state, and federal requirements, including, where applicable, the current United States Access Board Public Right-of-Way Accessibility Guidelines (PROWAG).

(4) The applicant **must keep the outdoor dining facility sanitary, neat and clean at all times,** free from accumulation of food, litter, snow, ice, and other potentially dangerous or unsanitary matter.

(5) The outdoor dining facility **must be in compliance with the Americans with Disabilities Act, 42 U.S.C. § 12101 et seq.** (as used in this section, the "ADA"), and at a minimum allow for five feet of continuous pedestrian access width along the public sidewalk free from all obstruction and must not create any pedestrian hazards. With the concurrence of the Public Safety Director, Police Chief, Fire Chief, Building Commissioner, Public Works Director, and City Engineer, the Planning Commission may allow a reduced minimum width of continuous pedestrian access to be permitted as allowed by the current PROWAG where the applicant can demonstrate that passing spaces will be available within the public right-of-way and within the frontage of the storefront, at the maximum interval of fifty feet, and have a minimum passing space recommended by the PROWAG that is free of all obstructions.



**(t) Outdoor Dining Facility.**

(6) An applicant whose restaurant use is at an intersection of public streets **shall not locate the outdoor dining facility in a manner that will impede vehicular sight distance at that intersection.** Setbacks from the intersection for the outdoor dining area will be determined by the Planning Commission on an individual basis specific to individual site conditions after review and recommendation by the Public Safety Director, Police Chief, Fire Chief, Building Commissioner, Public Works Director, and City Engineer.

**(7)** The **maximum number of seats for the outdoor dining facility will be determined by the Planning Commission** upon review of the amount of space available, compliance with minimum parking and other requirements of Zoning Code, the PROWAG, Building Code, and other applicable local, state, and federal requirements.

**(8)** **Applicants who serve alcoholic beverages** as part of their restaurant use must meet all requirements of the Ohio Department of Commerce, Division of Liquor Control, and the following standards:

A. Where an outdoor dining facility or any portion of an outdoor dining facility is located on public property, the owner of the facility shall **sign a use of public property agreement approved by the Director of Law** that indemnifies and holds the City harmless from any claims, liability or damages arising from the operation or location of the outdoor dining facility, and shall provide an insurance policy in an amount approved by the Director of Law with the City named as an additional insured. Upon approval of the outdoor dining facility conditional use by the Planning Commission, the Director is authorized to enter into the use of public property agreement on behalf of the City.

B. **A barrier on some or all sides of the outdoor dining facility may be required.** The Planning Commission will make a determination as to the extent and location of a barrier necessary for the outdoor dining facility. Any such barrier shall be in accordance with the Outdoor Dining Design Guidelines, Building Code, Ohio Fire Code, Ohio Department of Commerce, Division of Liquor Control requirements.

C. The **entrance to the outdoor dining facility must be easily recognizable and adjacent to or as close to a publicly used door of the restaurant use** as is commercially practicable in the Planning Commission's determination and comply with the Ohio Fire Code.

D. Unless authorized by the establishment of a designated outdoor refreshment area in accordance with Ohio Revised Code, **no person shall be permitted to carry alcoholic beverages from the outdoor dining facility to any place outside the outdoor dining facility except the adjacent restaurant use**, and the facility shall be designed in a way so as to maintain compliance with this provision.



**(t) Outdoor Dining Facility.**

**(9) Applicants who do not serve alcoholic beverages** as part of their restaurant use must meet the following standards:

A. Where an outdoor dining facility or any portion of an outdoor dining facility is located on public property, the owner of the facility shall **sign a use of public property agreement approved by the Director of Law** that indemnifies and holds the City harmless from any claims, liability or damages arising from the operation or location of the outdoor dining facility, and shall provide an insurance policy in an amount approved by the Director of Law with the City named as an additional insured. Upon approval of the outdoor dining facility conditional use by the Planning Commission, the Director is authorized to enter into the use of public property agreement on behalf of the City.

B. **A barrier on some or all sides of the outdoor dining facility may be required.** The Planning Commission will make a determination as to extent and location of a barrier necessary for the outdoor dining facility. Any such barrier shall be in accordance with the Outdoor Dining Design Guidelines and the Ohio Fire Code.

**(10)** The outdoor dining facility must have **adequate illumination during evening hours** in accordance with the Outdoor Dining Design Guidelines. All lighting will be designed to minimize the intrusive effect of glare and illumination upon abutting areas, especially residential properties pursuant to Section 1306.311.

**(11) Buffering with landscaping or fencing of the outdoor dining facility, if the facility is adjacent to residential or sensitive uses, may be required.** Buffering requirements will be determined by the Planning Commission and shall be in accordance with the Outdoor Dining Design Guidelines.

**(12)** An outdoor dining facility with a capacity of more than eight seats shall be included in the gross floor area square footage to determine parking space requirements per Chapter 1143.

**(13) Furniture and enclosures located within the outdoor dining facility may not be stored on the public right-of-way and must be removed entirely from the right-of-way on or before November 30 through March 31.** From December 1 through March 31, and in order to take advantage of unseasonable weather or special events, the Director shall have the authority to allow a single-day use of the public right-of-way in accordance with any approved conditional use permit. All furniture and fixtures used in an outdoor dining facility in the public right-of-way must be readily removable without damage to the surface of the right-of-way. **An outdoor dining facility located entirely on private property is permitted year-round with prior approval of the Planning Commission.**



**(t) Outdoor Dining Facility.**

**(14)** An outdoor dining facility shall **operate only between the hours of 8:00 a.m. and 10:00 p.m. Sunday through Thursday, 8:00 a.m. Friday to 1:00 a.m. Saturday and 8:00 a.m. Saturday to 1:00 a.m. Sunday.** The limitations set forth herein, and any additional limitations placed on hours of operation as may be determined by the Planning Commission, are designed to assure adequate peace, quiet and serenity in the evening and morning hours during which residents of the City are entitled to enjoy a period of rest and relaxation without intrusion from the possibilities of excessive noise and activity in adjacent areas. The Planning Commission may temporarily extend the hours and dates of operation of any outdoor dining facility where the outdoor dining facility is located within the footprint of a special event that is either sponsored by the City or permitted under the Codified Ordinances.

**(15) Outside entertainment,** whether by band, orchestra, instrument, musician, singer, radio, television, loudspeaker, microphone, recital or any other individual, group or mechanical device, shall only be permitted in an outdoor dining facility pursuant to Chapter 515 of the Ordinances of the City. The Planning Commission may prohibit or provide for other restrictions and conditions related to such entertainment as it deems necessary to protect the surrounding neighborhood. All speakers must be facing inward toward the outdoor dining facility. All outdoor entertainment must cease at 10:00 p.m.

**(16)** Unless otherwise approved by the Architectural Board of Review, **signage of any kind is not permitted on tables, umbrellas, chairs, or barriers** that are part of the outdoor dining facility.

**(17)** The outdoor dining facility **shall comply with the requirements of the state smoking and tobacco use laws.**

**(18)** The Director may require adjustments, after approval of the Planning Commission, to the layout, dimensions, or distance from the property line of any outdoor dining facility in order to ensure pedestrian safety and maintain shared use of the public right-of-way.



**(t) Outdoor Dining Facility.**

(19) Notwithstanding anything in this chapter or section to the contrary, any outdoor dining facility conditional use permit application shall be heard by the Planning Commission and, if approved, shall expire twelve months from the date of issuance. **Subsequent renewal of the conditional use permit may be made administratively by the Director if no significant modifications to the conditions of the permit have been proposed and no violations of the Code have been determined.** Determination of renewal status is at the discretion of the Director. The conditional use permit for an outdoor dining facility is non-transferable.

(20) Notwithstanding anything in this chapter or section to the contrary, any conditional use permit granted for the outdoor dining facility **may be revoked by the Planning Commission after referral to the Planning Commission by the Director and a public hearing.** Notice of such hearing shall be sent to the applicant and to others pursuant to Section 1173.07(b)(2) of this Code as if a zoning change were requested. The Director shall have the authority to cancel a use of public property agreement upon thirty days' written notice. Once a notice of cancellation of the use of public property agreement has been issued, the owner of the restaurant has five business days to remove any portion of the outdoor dining facility that is in the public right-of-way and restore the public right-of-way to the condition in which it existed prior to the creation of the outdoor dining facility.

(Ord. 33-2022. Passed 11-21-22.)

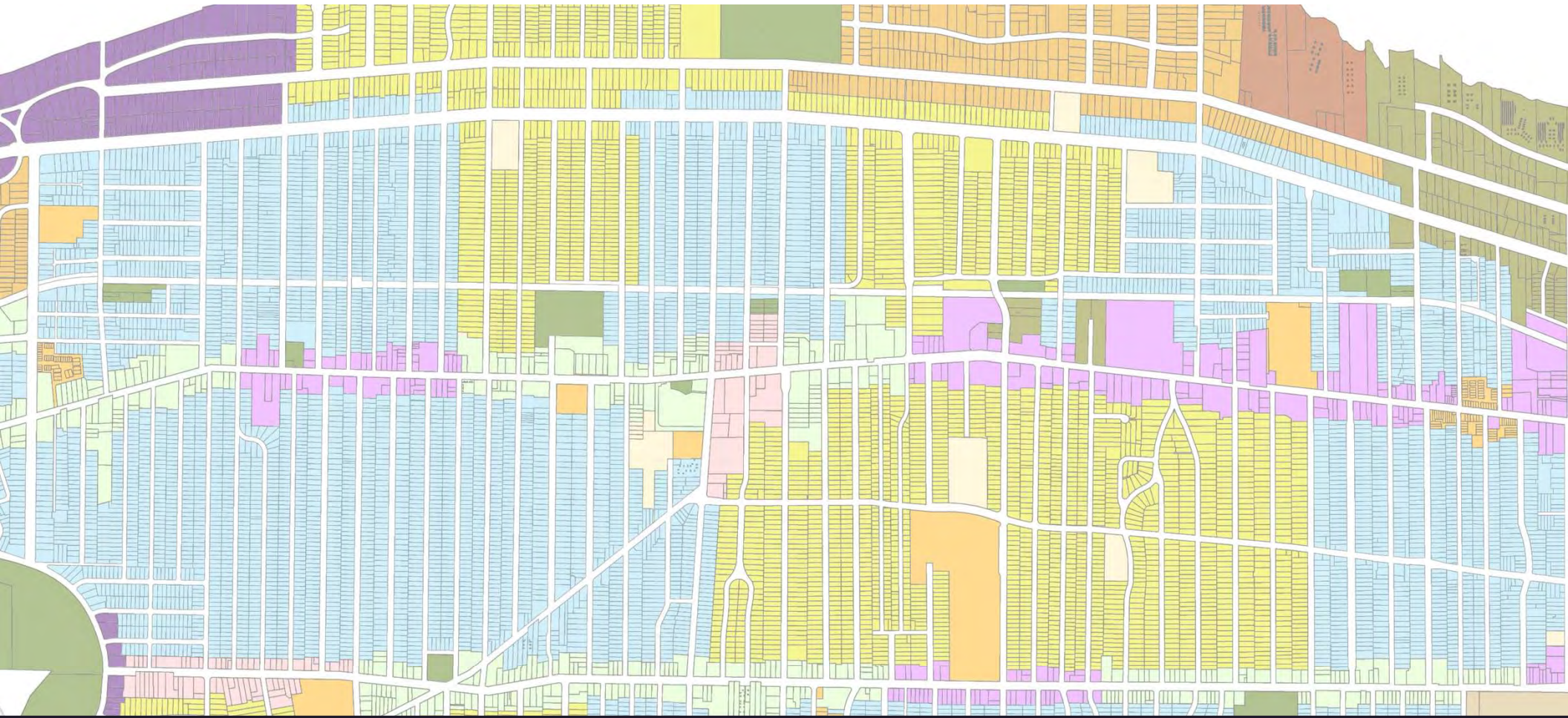


## **Recommendation (10-38-24) – Conditional Use (Outdoor Dining)**

Applicant needs to apply for approval by Architectural Board of Review (ABR)

Approval – conditional on ABR approval.





**Docket 07-30-24 – Complete Streets Initiative: Ordinance No. 11-2024**  
Communication for Review



## **Request (07-30-24)**

The review/amend of **Ord. 11-2024 (Complete Streets Initiative)** pursuant to:

- Chapter 901 – Streets and Sewers

**Legislation (Ordinance) being collectively combined/ revised – DEFER**



12650 DETROIT AVENUE 44107 216-529-6055  
[www.lakewoodoh.gov](http://www.lakewoodoh.gov)  
Lakewood City Council  
SARAH KEPPLER, PRESIDENT  
JASON SHACHNER, VICE PRESIDENT

**Council at Large**  
THOMAS R. BULLOCK III  
TRISTAN RADER  
SARAH KEPPLER

**Ward Council**  
KYLE BAKER, WARD 1  
JASON SHACHNER, WARD 2  
CINDY STREBIG, WARD 3  
CINDY MARK, WARD 4

June 3, 2024

**Re: Ordinance to Enact Section 901.23 Complete Streets of the Lakewood Codified Ordinances**

Dear Colleagues,

On April 15, 2024, City Council adopted Resolution 2023-73 that adopted the Active Transportation Plan and accompanying Safe Streets for All Plan (collectively, the "Active Transportation Plan") which called for the adoption of a Complete Streets policies and legislation to ensure accountability measures for implementation. The Active Transportation Plan had the following Community Vision Statement:

Lakewood envisions a complete and connected network for people walking and biking that provides year-round access to local amenities, resources, and the regional bicycle and pedestrian network. Lakewood envisions a culture of respect for all roadway users, and bicycle and pedestrian infrastructure that is safe and comfortable for people of all ages and people with disabilities.

It is in line with the Active Transportation Plan and its bold Community Vision Statement that we bring forward this Ordinance to enact Section 901.23 Complete Streets of the Lakewood Codified Ordinances, to address the livability and safety needs of Lakewood by building upon efforts to promote an equitable multimodal transportation system. The Complete Streets Ordinance sets up an accountability structure to ensure that the City is meeting the laudable goals outlined in the Active Transportation Plan.

Moreover, the City and our School District are unique in that we do not provide school buses to bring students to school because we have neighborhood schools where most elementary and middle school students live within walking and/or biking distance to their respective school. It has proudly been called a "Walking School District." The Active Transportation Plan provided data that the key issues impacting caregiver decisions to let a child walk or bike to school included "safety of intersections and crossings" at 61%, "speed of traffic along route" at 45%, and "convenience of driving" at only 13%. As such, the convenience of driving is not driving those decisions. Safety is. Moreover, 67% of fatal or serious injury pedestrian crashes and 40% of fatal or serious injury bicycle crashes happened within 1/4 miles of schools and over 83% and 90% occurred within a 1/2 mile of schools. To be blunt, we owe it to the most vulnerable among us to do all we can to implement the safety infrastructure improvements outlined in the Active Transportation Plan, the Americans with Disabilities Act Transition Plan (adopted

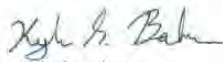
November 20, 2023), and the Climate Action Plan (adopted May 1, 2023). Achieving the goals of these rich and complex documents will require a long-term intersectional approach designed to last beyond any one Council or Administration.

While many communities that enact Complete Streets Ordinances create a new volunteer committee or task force to oversee its implementation, the City already has a qualified body in the Planning Commission to take on the work. Section 7.2 of Lakewood's Third Amended Charter ("Charter") creates and provides structure to Lakewood's Planning Commission. The Planning Commission is entrusted with many responsibilities under the Charter, but Section 7.2(g) outlines items that require "mandatory referral" to the Planning Commission. That section outlines that "nor shall any street be opened, widened, narrowed, relocated or vacated, or its use changed for any purpose whatsoever ... unless it has first been submitted to [Planning Commission] for report and recommendation." Considering the Planning Commission's Charter-bound duty to be consulted on any changes to our public rights of way and the goals of the Complete Streets Ordinance, they are well-suited and pre-positioned to fill this role. Moreover, besides having the expertise to do the work, the Planning Commission also has the capacity as they have gone from a high of 85 agenda items in 2017 to only having 37 in 2022 and 25 in 2023. See Planning & Development Department FY 2024 Budget Proposal.


The Complete Streets Ordinances seeks to have the City, before embarking on any transportation project, to come before Planning Commission and show the treatments called for in the Active Transportation Plan for each transportation project. If those treatments are not feasible, then the City must explain why in a public meeting before Planning Commission. This process will increase public participation, safety, and ensure that the City meets the lofty goals outlined in the Active Transportation Plan. We understand that this proposal creates a process that was not traditionally included in the City's process of embarking on transportation projects and will require established protocols to be modified to accommodate the public input and accountability structure of the Active Transportation Plan, but we could think of no better reason than the safety of our most vulnerable users of our public rights of way to endeavor to make those changes. We look forward to working with the Administration, our colleagues, and the public on the Complete Streets Ordinance to create a safer and more equitable Lakewood.

We respectfully ask that this correspondence and the accompanying Complete Streets Ordinance be simultaneously referred to Planning Commission and the Committee that Council deems most advisable.

Sincerely,

  
Kyle G. Baker  
Councilmember Ward 1

  
Sarah Kepple  
City Council President

  
Cindy Strebig  
Councilmember Ward 3

AN ORDINANCE to take effect immediately provided it receives the affirmative vote of at least two thirds of the members of Council to enact Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood, to address the livability and safety needs of Lakewood by building upon efforts to promote an equitable multimodal transportation system;

WHEREAS, the main objectives of Complete Streets are to design, build, and maintain roads, including multi-use trails, that safely and comfortably accommodate all users of roadways of all ages and abilities, including pedestrians, bicyclists, shared mobility device users, motorcyclists, public transit and school bus riders, motorists, delivery and service personnel, freight haulers, and emergency responders;

WHEREAS, on April 15, 2024, City Council adopted Resolution 2023-73 that adopted the Active Transportation Plan and accompanying Safe Streets for All Plan which called for the adoption of a Complete Streets policies and legislation to ensure accountability measures for implementation;

WHEREAS, integrating sidewalks, low stress bicycle facilities, public transit amenities, and safe crossings in the initial design of a project spares the expense and complications of retrofits later;

WHEREAS, the City of Lakewood is committed to the creation of a network of Complete Streets that will encourage economic growth, increase property values, eliminate serious and fatal crashes through safety improvements, improve public health and fitness, reduce harmful emissions, and reduce the overall demand on or roadways by allowing people to replace motor vehicle trips with sustainable transportation options while also improving the environmental and social well-being of citizens;

WHEREAS, the desired outcome of the Complete Streets Ordinance is to create an equitable, balanced, and effective transportation system that prioritizes access and safety for the most vulnerable road users, where every roadway user can travel safely and comfortably, and where sustainable transportation options are available to everyone;

WHEREAS, the desired outcome of Complete Streets Ordinance will enable access to destinations such as schools, parks, healthy food retail establishments, public transit, and other destinations that are critical for health equity;

WHEREAS, the goals of this Complete Streets Ordinance is to create a comprehensive, integrated, and connected transportation network that supports dense, sustainable development and provides livable communities; to ensure safety, ease of use, and ease of transfer between modes for all users of the transportation system; and to provide context sensitive design flexibility for different types of streets, areas and users;

WHEREAS, the Complete Streets Ordinance is critical to achieving the goals of the Vision Zero commitment, which seeks to eliminate deaths and serious injuries from traffic crashes (City Council passed Resolution 19-9063 in 2019 in favor of achieving Vision Zero);

WHEREAS, the Complete Streets Ordinance is integral to achieving the goals of Lakewood's Climate Action Plan, which seeks to reduce harmful air emissions from the transportation sector, among other measures;

WHEREAS, other cities nationwide, including Cleveland, Pittsburgh, Baltimore, Phoenix, and Baton Rouge, have adopted Complete Streets policies and legislation in furtherance of these guiding principles; and

WHEREAS, as set forth in Section 2.12 of the Third Amended Charter of the City of Lakewood, this Council by a vote of at least two thirds of its members determines that this ordinance is an emergency measure and that it shall take effect immediately and that it is necessary for the immediate preservation of the public property, health, and safety; now, therefore

BE IT ORDAINED BY THE CITY OF LAKEWOOD, OHIO:

Section 1. That new Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood is hereby enacted, reading as follows:

Section 1. That new Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood is hereby enacted, reading as follows:

Section 1. Definitions

For purposes of this ordinance, the following terms, not defined in the Codified Ordinances of the City of Lakewood, shall mean:

- (a) “Active Transportation” means being physically active for the purpose of transportation (typically biking or walking), and is distinct from being physically active for recreation.
- (b) “Active Transportation Plan” shall mean the Safe Streets for Lakewood Active Transportation Plan and Safe Streets for Lakewood Safety Action Plan adopted by Lakewood City Council on April 15, 2024 pursuant to Resolution Number 2023-73. The Active Transportation Plan shall be updated at least once every 10 years.
- (c) “Complete Streets” means the City’s commitment to ensure that, when possible:
  - a. Roads safely and comfortably accommodate users including pedestrians, bicyclists, mobility device users, motorcyclists, public transit users and motorists; and
  - b. Roadway projects add to the prevalence of trees and vegetation across Lakewood, as well as best practices for stormwater management; and
  - c. Roadway projects use sustainable and recycled materials.
- (d) “Complete Streets Policy” means the guidance and procedures developed by the Director of Public Works and the Director of Planning and Development (or their respective designees) that address the inclusion of Complete Streets elements in all types of projects within in the public right-of-way, including new construction, reconstruction, rehabilitation, repair, restriping, and maintenance of transportation facilities and development projects and can include the Active Transportation Plan (and any further update to the Active Transportation Plan approved by City Council).

(e) “Connectivity” means the level to which travel routes are safe, accessible, and convenient for road users to use across varying distances without gaps.

(f) “Planning Commission” means the Lakewood Planning Commission as defined and authorized in Section 7.2 of the Third Amended Charter of the City of Lakewood.

(g) “Safe design” means with the intent of protecting all road users from death or injury.

(h) “Transportation Projects” mean any project for which the City is the project sponsor and is responsible for providing the scope of work including new construction, reconstruction, rehabilitation, repair, resurfacing, restriping and maintenance of transportation facilities and development projects.

(i) “Vision Zero” means a strategy to eliminate all traffic fatalities and severe injuries, while increasing safe, healthy, equitable mobility for all

(j) “Vulnerable road users” means road users at risk due to lack of external protections, including pedestrians, cyclists, scooter riders, motorcyclists, and those with mobility, vision, and/or hearing challenges.

Section 2. That the Director of Public Works and the Director of Planning and Development (or their designees) shall plan, prioritize, implement, and enforce policies and guidelines related to Complete Streets in all types of Transportation Projects within the public right-of-way. Unless plans to individually construct new Complete Streets elements exist, the implementation of Complete Street elements shall be evaluated for all types of Transportation Projects within the public right-of-way.

Section 3. That the design of capital investments and Transportation Projects shall prioritize the most vulnerable road users and follow transportation standards and guidelines as adopted by the Director of Public Works and Director of Planning and Development to align with the intent of Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood. The standards and guidelines shall be made publicly available on the City's website and can include the current City Council approved version of the Active Transportation Plan. The City shall use the best and latest design guidelines, standards, and recommendations available when considering methods or providing development flexibility within safe design parameters and balanced design solutions between user and modal needs. A balanced approach considers aspects such as street design and width, curb bump outs, raised crosswalks, desired operating speed, hierarchy of streets, connectivity, wayfinding signs and signal variation from a human scale for the needs and comforts of all users.

The City shall consider innovative or non-traditional design options where accepted design standards allow flexibility. Design criteria shall be based on the thoughtful application of engineering, architectural, and urban principles in addition to prescriptive guidelines. Best practices related to street design, construction, and operations can be found in, but are not limited to: Standards, including the following: The Ohio Manual of Uniform Traffic Control Devices (OMUTCD), The Policy on Geometric Design of Highways and Streets ("Green Book"), and the Americans with Disabilities Act (ADA) Standards for Accessible Design; Guidelines, including the following: Publications from ODOT, including the Location and Design Manual, Publications from the American Association of Highway Transportation Officials (AASHTO), Publications from the National Association of Transportation Officials (NACTO), including the Urban Street design guides, publications from the Transportation Research Board, including the Highway Capacity Manual, NOACA's Street Design Guidelines and Trail Crossing Typology Guidance, Cuyahoga County Complete Streets Toolkit, and processes, policies, and guidance adopted through the Vision Zero Taskforce; and Plans adopted by the Lakewood Planning Commission, including the following: the 2019 Community Vision (updated with recommendations from the Resiliency Task Force), the Climate Action Plan, the ADA Transition Plan, and Active Transportation Plan, as well as in-progress and future planning efforts.

Section 4. That Complete Streets elements on Transportation Projects conducted by the City that do not affect sub base, curbs and sidewalks shall be limited to minimal impact elements if it has been determined through the advisory process outlined in Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood that additional elements are unnecessary or infeasible. Minimal impact elements that may be included are pavement markings and lane reconfiguration to accommodate bicycle facilities and/or public transit lanes, signage for cyclists or pedestrians and the use of sustainable materials. The Planning Commission shall be the body that oversees and provides advisory guidance on the Complete Streets Policy.

Section 5. By January 1, 2025, the Public Works Department and Planning and Development Department shall coordinate with the Planning Commission to adopt a process for capital improvement prioritization and project review based on the Vision Zero Guidelines and the Active Transportation Plan, and guided by values established in 2019 Community Vision (updated with recommendations from the Resiliency Task Force), and other road-way specific plans adopted by the City. The Planning Commission shall use said process to maintain a corridor-specific Complete Streets implementation list that identifies applicable Complete Street elements for priority corridors to transmit to the Department of Public Works to integrate into project scopes and to City Council for review prior to the final authorization of funding for said Transportation Project. This implementation list can include the current list outlined in the most current version of the Active Transportation Plan. This timeline shall be incorporated in the early stages of street design as not to hinder project timeline. Projects that are not able to accommodate the Complete Streets elements specified by the Planning Commission shall follow the formal exemption process outlined in Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood.

The Planning Commission shall review and provide feedback on project scope and design for all Transportation Projects in the City and consider exemption requests as outlined in Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood.

The Planning Commission shall conduct a post-construction evaluation of completed Transportation Projects to assess effectiveness and collect community feedback for the annual report.

The Planning Commission shall review this Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood regularly and propose any necessary revisions (if revisions are deemed necessary).

Section 6. That exemptions for Transportation Projects to be considered include, but are not limited to, the following:

- (a) The Transportation Project involves a roadway on which non-motorized use is prohibited by law. In this case, an effort shall be made to accommodate active transportation users elsewhere;
- (b) Where the cost of accommodation is excessively disproportionate to the need or probable use. Financial hardship exists when compliance with Complete Streets policies and guidelines constitutes a minimum of twenty (20) percent of the total project cost;
- (c) Severe topographic or natural resource constraints;
- (d) There is documentation that there is an absence of use by all except motorized users now and in the future even if the street were a Complete Street;
- (e) Public transit enhancements when the street is not on a public transit route; and
- (f) The project scope and/or design does not meet the Complete Streets elements identified as relevant by the Planning Commission in the Complete Streets implementation list.

The Director of Public Works and Director of Planning and Development shall provide notice of all exemption requests to the Planning Commission and City Council. The Public Works and Planning and Development Departments must submit documentation to the Planning Commission indicating why the Transportation Project should be exempt. The documentation shall include how the Transportation Project will accommodate roadway users who will be adversely impacted by the exemption request.

Within 60 days of receiving an exemption request, the Planning Commission will review the exemption request, and provide comments to the Director of Public Works and Director of Planning and Development for determination, and the exemption request shall be made publicly available on the City's website. The Planning Commission shall hold at least one public hearing to receive and consider public input, which public hearing may be held as part of a regular monthly Planning Commission meeting. If the Planning Commission rejects the exemption request, it shall go to City Council with 30 days for a final determination. If the Planning Commission accepts the exemption request by a majority vote of the present members, it is formally accepted.

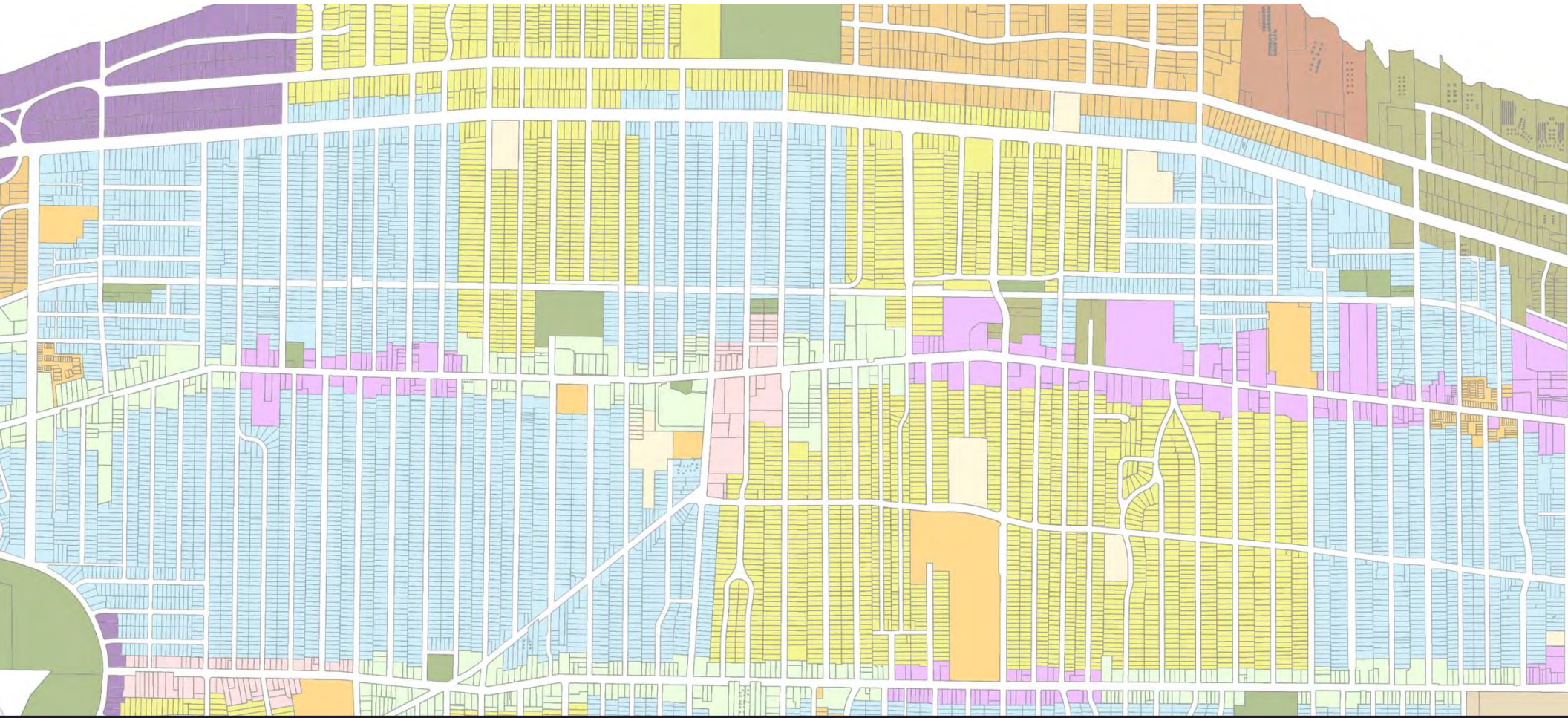
Section 8. That all Transportation Projects approved under the Complete Streets Policy include provisions for an ongoing maintenance plan of the improvements. Alternative maintenance arrangements may be utilized to reduce the costs to the City for ongoing maintenance, such as maintenance agreements with adjacent property owners.

Section 9. That the Director of Public Works, and the Director of Planning and Development, with the advice and input from the Planning Commission shall provide an annual progress report to City Council, made readily available to the public, on the implementation of the Complete Streets Policy. The initial annual progress report shall be presented before Council on or about June 30, 2026. Thereafter, the annual report shall be prepared by the end of the second quarter of the following year and outline the previous year's accomplishments and list expected projects for the upcoming year. The annual report shall also serve to evaluate the performance and execution of the Complete Streets Policy. Evaluation of performance may be described by and shall include the following performance measures, if available:

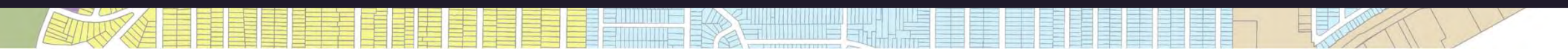
- (a) Use data by transportation mode;
- (b) Pre- and post-project speed/crash data;
- (c) Feedback from the community;
- (d) Linear feet of sidewalks built and repaired;
- (e) Number of ADA accessible curb ramps built;
- (f) Miles of bicycle facilities built;
- (g) Number of bicycle amenities built by type;
- (h) Number of public transit accessibility accommodations built by type;
- (i) Number of curb bump outs installed;
- (j) Number and type of crosswalk and intersections improvements;
- (k) Number of public hearings held for Complete Streets projects;
- (l) Number of exemptions from Complete Streets Policy approved and for what reason; and
- (m) Other relevant information from the Planning Commission's post-construction evaluation of completed Transportation Projects.

These measures must be compiled in partnership with relevant departments, including Public Works and Planning and Development, and reported to City Council and made available to the public on an annual basis.

Section 10. That this ordinance shall take effect and be in force from and after the earliest period allowed by law.



**Docket 07-31-24 – Complete Streets Initiative: Resolution No. 2024-42**  
Communication for Review



## **Request (07-31-24)**

The review/amend of **Resolution No. 2024-42 (Complete Streets Initiative)** pursuant to:

- Chapter 901 – Streets and Sewers

**Legislation (Ordinance) being collectively combined/ revised - DEFER**



BE IT ORDAINED BY THE CITY OF LAKEWOOD, OHIO:

Section 1. That new Section 901.23 Complete Streets of the Codified Ordinances of the City of Lakewood is hereby enacted, reading as follows:

Section 1. Definitions

For purposes of this ordinance, the following terms, not defined in the Codified Ordinances of the City of Lakewood, shall mean:

- (a) "Active Transportation" means being physically active for the purpose of transportation (typically biking or walking), and is distinct from being physically active for recreation.
- (b) "Active Transportation Plan" shall mean the Safe Streets for Lakewood Active Transportation Plan and Safe Streets for Lakewood Safety Action Plan adopted by Lakewood City Council on April 15, 2024 pursuant to Resolution Number 2023-73. The Active Transportation Plan shall be updated at least once every 10 years.
- (c) "Complete Streets" means the City's commitment to ensure that, when possible:
  - a. Roads safely and comfortably accommodate users including pedestrians, bicyclists, mobility device users, motorcyclists, public transit users and motorists; and
  - b. Roadway projects add to the prevalence of trees and vegetation across Lakewood, as well as best practices for stormwater management; and
  - c. Roadway projects use sustainable and recycled materials.
- (d) "Complete Streets Policy" means the guidance and procedures developed by the Director of Public Works and the Director of Planning and Development (or their respective designees) that address the inclusion of Complete Streets elements in all types of projects within in the public right-of-way, including new construction, reconstruction, rehabilitation, repair, restriping, and maintenance of transportation facilities and development projects and can include the Active Transportation Plan (and any further update to the Active Transportation Plan approved by City Council).
- (e) "Connectivity" means the level to which travel routes are safe, accessible, and convenient for road users to use across varying distances without gaps.
- (f) "Planning Commission" means the Lakewood Planning Commission as defined and authorized in Section 7.2 of the Third Amended Charter of the City of Lakewood.
- (g) "Safe design" means with the intent of protecting all road users from death or injury.
- (h) "Transportation Projects" mean any project for which the City is the project sponsor and is responsible for providing the scope of work including new construction, reconstruction, rehabilitation, repair, resurfacing, restriping and maintenance of transportation facilities and development projects.
- (i) "Vision Zero" means a strategy to eliminate all traffic fatalities and severe injuries, while increasing safe, healthy, equitable mobility for all
- (j) "Vulnerable road users" means road users at risk due to lack of external protections, including pedestrians, cyclists, scooter riders, motorcyclists, and those with mobility, vision, and/or hearing challenges.



**Docket 10-37-24 – Donald & Thoreau  
Planned Development - Residential**



## **Request (06-21-24)**

To review and approve a **Planned Development (Final Plan)** pursuant to:

- Chapter 1156 – Planned Development.

Proposal is in the R2 - Residential, Single and Two Family, District.

### **Preliminary Approval Conditions**

The Commission wanted it on record the letters received prior to the meeting were all in opposition of the development; the Commission was sensitive to the concerns. Mr. Baas provided administrative comments and outlined the City's recommended conditions of approval. ABR review had started.

A motion was made by Mr. McDermott, seconded by Mr. Sanderson to **APPROVE Docket No. 06-21-24 for a Preliminary Planned Development Review with the following conditions:**

1. **Continue to refine/develop the northern buffer – including communication with adjacent property owners.**
2. **Continue to study/refine the private access drive – recommendation is for resident access to be from Thoreau with restricted access/egress (Fire Dept.) from Donald side.**
3. **Continue to study/coordinate easement or other method to dedicate sidewalk for public use.**

All the members voted yea; the motion passed.



1. Introduction
2. Developer Presentation
3. City Analysis
4. Planning Commission questions & feedback
5. Public comment & questions
6. Discussion on comments & questions

*Points to be addressed (from October meeting) were:*

- *The buffer.*
- *Provide a plat, at least a working one.*
- *Placing the eight-foot-wide path in a public easement for public use as a sidewalk.*
- *Provide more information on tree replacement ratio.*
- *Provide a full-blown landscape plan with samples.*
- *One-way directional flow of the driveway, from Thoreau Avenue to Donald Avenue, with signage and potentially **bollards**.*



# Developer Presentation



ENLARGED AERIAL

NTS



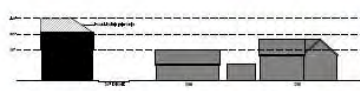
CONTEXT ELEVATIONS - DONALD AVENUE

NTS



THOREAU TO SOUTH

NTS



CONTEXT ELEVATIONS - THOREAU AVENUE

NTS



THOREAU FROM NORTH

NTS



VIEW TO WEST

NTS



THOREAU TO NORTH

NTS



ACROSS THOREAU

NTS

ISSUE	DATE	DESCRIPTION
1	05.20.24	schematic design
2	07.18.24	abr & planning review
3	09.18.24	abr & planning review
4	10.23.24	abr & planning review



PAUL R. BEEGAN, LICENSE #12574  
EXPIRATION DATE: 12.31.2025

**BEEGAN**  
ARCHITECTURAL DESIGN

15703 MADISON AVENUE, 216.521.9000 PHONE  
LAKEWOOD, OHIO 44107, 216.916.4591 FAX  
WWW.BEEGAN-AD.COM, PAUL@BEEGAN-AD.COM

project no. 23-075

Hilane Realty  
Donald & Thoreau  
Development

ppn 312-26-102  
lakewood, oh 44107

Site Context

**A0**

## 1156.01 PURPOSE.

(a) Planned Development (PD) zoning is intended to encourage orderly development and redevelopment of property, while **allowing more flexibility and creativity in design** to achieve **high quality, integrated site planning** not otherwise possible under the constraints of normal zoning requirements without detriment to neighboring properties.

PDs are intended to permit a more flexible approach to land use control and to promote development that is innovative, integrated with surrounding uses, and shows sensitivity to cultural, environmental, and economic considerations

(b) Planned Development Zoning is intended to **encourage development which is consistent with the Community Vision** including more compact development, pedestrian-friendly site design, urban street character, energy-efficient design, industry best practices, and accommodation of a range of compatible land uses through appropriate site design. PDs are intended to permit a more flexible approach to land use control and to promote a variety of housing types.

(c) A PD encourages the **development of compact, pedestrian-scaled, neighborhoods while providing greater efficiencies in use of infrastructure**. It is intended to help advance revitalization initiatives and recognize the market demand for new residential and commercial development within compact, pedestrian friendly districts.

PD zoning is intended to **work in conjunction with the proactive development** of pocket parks, open spaces, and the creation of public spaces within the districts. PD Zoning specifically discourages those uses that: promote a strip center development pattern, promote idle land and over parking, and detract from the image enhancement intentions of this district.

(Ord. 17-17. Passed 3-20-17.)



## 1156.02 LOCATION OF PLANNED DEVELOPMENTS.

(e) A PD should utilize the **following characteristics to the greatest possible extent which shall be considered in the approval process:**

- (1) Designs that **reflect the Vision and that offer types or densities of development that are not available under the other Sections of this Code.**
- (2) Designs that **utilize a creative approach to achieve better urban design, efficiencies in use of land and infrastructure, and the provision of aesthetic amenities.**
- (3) Designs that **provide appropriate buffers and transitions** between areas with different land uses and development densities.
- (4) Designs that **maintain or enhance the appearance of neighborhoods** by complementing neighborhood architectural character.
- (5) Designs that are intended to **encourage flexibility, innovation, and creativity in site and development design** by allowing the mixing of permitted uses and/or modification of variation from otherwise applicable zone district and development standards.
- (6) Designs that **conserve areas of natural beauty and green spaces to the greatest degree possible** consistent with accommodating new development.
- (7) Designs that **incorporate "green architecture" pursuant to Resolution 7746-03...City...Green Building Policy.**

(Ord. 17-17. Passed 3-20-17.)



## 1156.04 PLANNED DEVELOPMENTS.

### (e) Procedures for Approval of a Final PD Plan.

- (1) **A plan previously receiving preliminary approval may be submitted for Final Approval.**
- (2) A Final PD Plan may cover the entire area covered by the Preliminary PD Plan, or it may include only a phase or phases of the Preliminary PD Plan. Only such phases as receive Final Approval may be advanced for purposes of obtaining building permits and being constructed.
- (3) A preliminary plan...and a plat complying with the requirements set forth in Section 1155.03, Subdivision Regulations, General Requirements, shall be prepared...**The purposes of the preliminary plan are to allow the Commission to determine if it qualifies as a potential PD and its compliance with other City codes.**
- (4) Concurrent with submission of an application for approval of a Final PD Plan, the Director shall submit to City Council a final Development Agreement, if applicable, for review and execution.
- (5) **The Commission's role shall be to review any application for Final PD Plan approval and make a recommendation to the Director to approve, approve with conditions, or deny the application based on its compliance with Section 1156.03 of this Code.**
- (6) If the Commission recommends approval of an application with conditions, the applicant shall resubmit to the Commission a revised Final PD Plan that reflects the changes or modifications required or suggested by the Commission in its initial review.
- (7) Upon receipt of the approved Final PD Plan from the Commission the **Director shall forward the application to the Architectural Board of Review for review of the application based on compliance with the standards set forth...(Sections 1156.05, 1325).**
- (8) Upon receipt of:
  - A. The approved Final PD Plan from the Architectural Board of Review,
  - B. An approved lot split and/or lot consolidation plat signed by the Chairman and Secretary of the Commission, and
  - C. An approved and executed Development Agreement from City Council if applicable, the Final PD Plan and plat shall be forwarded to Council along with the Commission's recommendation for approval.
  - D. Council shall approve or disapprove the Final PD Plan within forty-five (45) days from the date received by Council.
  - E. When the Final PD Plan is approved (plat recorded, permits applied for/issued, construction begins...)



### 1156.03 STANDARDS FOR REVIEW OF A PRELIMINARY PD PLAN.

An application for approval of a Preliminary PD Plan, together with submitted plans and reports, shall be reviewed for its conformance with the following standards:

- (a) ...sensitive to the abutting land uses and to the zoning designation which the PD is replacing. Mixed use within the planned development or within the same structure located in the PD is encouraged where appropriate;
- (b) ...comply with the subdivision requirements as set forth in Chapter 1155 of this Code, except to the extent modifications, variances, or waivers have been expressly allowed pursuant to paragraph (e) below;
- (c) Adverse impacts on adjacent properties, including but not limited to increased traffic or noise, as described in Chapter 515 of the Ordinances, and visual impacts, shall be mitigated to the maximum extent feasible;
- (d) ...integrated with adjacent development through street connections, sidewalks, trails, and similar features;
- (e) All district, development, and subdivision standards...may be modified or varied upon a finding that the proposed PD incorporates creative site design which represents an improvement in quality and service of the purposes set forth in Section 1156.01...including but not limited to improvements in open space provision and access; environmental protection; tree/vegetation preservation; efficient provision of streets, roads, and other utilities and services; or choice of living and housing environments;
- (f) ...shall comply with the following requirements, which shall not be modified or varied except as expressly set forth below or as permitted by the Commission.
  - (1) Minimum area requirement. All Planned Developments shall have a minimum size of 10,000 square feet.
  - (2) Setbacks from adjoining residential uses. A Planned Development shall comply with any applicable zone district standards that require minimum setbacks from adjoining residential uses or properties as set forth in Chapter 1121 and 1123.
  - (3) Environmental protection standards. All Planned Developments shall comply with Ohio Environmental Protection Agency regulations and local ordinances.
  - (4) Architectural and design standards. All Planned Developments shall comply with all architectural and design standards, including those set forth in Section 1156.05, Design Principles, of this Code and Chapter 1325 of the Building Code.



# Roles & Responsibilities

## Planning Commission

### Land Use/Zoning:

- ❑ Standards for Preliminary Review set forth in 1156.03
  - ...sensitive to the abutting land uses...mixed use is encouraged (use table and area map).
  - ...comply with the subdivision requirements as set forth in Chapter 1155 (final plat).
  - Adverse impacts on adjacent properties, including but not limited to increased traffic or noise...(traffic impact)
- ❑ (1156.05) PD Design Principles:
  - Building (Structure Height, FAR) and Site (Lot Size, Setbacks)
  - Pedestrian Access and Circulation
  - Vehicular Circulation and Access (Traffic Impact Study)
  - Parking (quantity)
  - Streetscape improvements + Landscaping.

## Architectural Board of Review

### Building Orientation/Design/Materials:

- ❑ Standards for review set forth in 1156.05 and Chapter 1325 of the building code.
  - (1325)...by regulating, according to accepted and recognized architectural principles, the design, use of materials, finished grade lines, dimensions, orientation and location of all...buildings to be erected...(including Commercial District and other guidelines)
  - (1156.05) PD Design Principles
    - Building and Site (Massing/Design)
    - Pedestrian Access and Circulation
    - Vehicular Circulation and Access
    - Parking (Visual impact, 1325.08/Design)
    - Streetscape improvements + Landscaping.
    - Building Materials
    - (Finalize) Landscaping
    - (Finalize) Streetscape improvements
    - Service Area and Mechanical Screening
    - Signage (Master sign plan required)
    - Lighting (...including photometric illustration)



## 1156.05 DESIGN PRINCIPLES. (1 OF 6)

The following Design Principles provide certain guidelines and requirements, as noted, in the design preparation of a Preliminary PD Plan.

### (a) **Building and Site Design.**

- (1) Wherever feasible, buildings shall be designed to provide massing configurations with a variety of different wall planes. Plain, monolithic structures with long walls and roof plane surfaces are discouraged.
- (2) Building facades should incorporate design elements such as changes in color or texture; projections, recesses, and reveals; arcades or pergolas providing pedestrian interest; or equivalent elements that subdivide the wall into human scale proportions. Blank facades are highly discouraged, especially on walls facing streets or pedestrian ways.
- (3) Commercial Building facades shall have highly visible customer entrances that feature canopies, overhangs, arcades, distinctive roof forms, arches, display windows, or landscaped features. Primary entrances should face streets on which they are located.
- (4) Buildings shall have well defined rooflines with attention to architectural detail. Consideration should be given to the prevailing pattern of roofs in the area surrounding and within the PD.
- (5) Sloping roofs, where used, shall have one (1) or more of the following architectural features: gables, hips, horizontal or vertical breaks, or other similar techniques that are to be integrated into the building architecture.

(b) **Building Materials.** Building materials shall be limited to brick, masonry, stucco, wood, fiber, cement siding, wood shingle, wood siding, cultured stone, or other similar materials. Prohibited materials include aluminum or vinyl siding, dryvit-type products on the lowest eight (8) feet of any structure, split faced block, and other similar materials.



## 1156.05 DESIGN PRINCIPLES. (2 OF 6)

### (c) Vehicular Circulation and Access.

- (1) Circulation systems shall be designed to efficiently facilitate traffic flow, yet designed to discourage speeds and volumes that impede pedestrian activity and safety.
- (2) Street designs are encouraged to incorporate traffic calming devices and techniques.
- (3) Common or shared access points are encouraged.
- (4) To the maximum extent feasible, common or shared service and delivery access shall be provided between adjacent parcels or buildings, and provided to the rear of buildings.
- (5) Safe and adequate site distances shall be provided at all intersections.
- (6) Transit stops should be incorporated into site plans, where feasible.
- (7) The developer as part of the site plan review process shall provide traffic impact studies.

### (d) Pedestrian Access and Circulation.

- (1) A coordinated pedestrian system shall be provided throughout the PD, including connections between uses on the site, and between the site and adjacent properties and rights-of-way. Pedestrian connections shall be provided to properties across streets wherever feasible.
- (2) The site shall be connected to adjacent properties and pedestrian facilities to the maximum extent feasible.
- (3) Continuous sidewalks or other pedestrian facilities shall be provided between the primary entrances to buildings, all parking areas that serve the buildings, pedestrian facilities on adjacent properties that extend to the boundaries shared with the PD, any public sidewalk along perimeter streets, or other community amenities or gathering spaces.
- (4) Decorative sidewalks, such as brick pavers, are encouraged at key intersections or streets.
- (5) Street furniture or other amenities are encouraged, such as plazas, benches, and decorative pedestrian light fixtures.
- (6) Open and public areas should be provided as a mixture of green space landscaping and hardscape pedestrian areas with a goal of twenty (20) percent of the site area.



## 1156.05 DESIGN PRINCIPLES. (3 OF 6)

### (e) Parking.

- (1) Adequate parking shall be provided, but excessive parking is discouraged. The standards contained in Chapter [1143](#) shall be used as a guide, but **those standards may be modified without the need for a variance based upon other considerations as determined by the Commission**, and a finding by the Commission that the modified parking standards would comply with the provisions of the Code and the intent of the PD.
- (2) The visual impact of parking shall be minimized through the use of interior landscaped islands and through dividing parking spaces into groupings.
- (3) The edges of parking lots shall be screened through landscaping or other methods such as decorative fences.
- (4) **A minimum of one (1) off-street parking space shall be required behind each residential unit or garage. No garage openings shall be permitted onto public streets.**

### (f) Landscaping and Screening.

- (1) It shall be the duty of the Commission to determine whether a reasonable percentage of the area within a PD shall be maintained in a combination of landscaped and urban open space. The project must adhere to the spirit of the City's landscape values. The standards contained in Chapter [1141](#) shall be used as a guide, but those standards may be modified without the need for a variance based upon other considerations determined by the Commission that such considerations would comply with the provisions of the Code and the intent of the PD.
- (2) **Pedestrian access from adjacent residential streets is encouraged.** The owners of **residential property directly abutting rear yards, parking and loading areas of a PD shall be contacted and offered masonry screening and/or appropriately designed alternatives.** PD applicants shall document meetings with abutting residential owners and the results of such meetings. The intent of this provision is for the applicant to involve nearby residents in the PD project. City staff shall assist in this process.
- (3) **Where required, screening fences and walls shall be erected.** The standards of Chapter [1141](#) shall be used as a guide, but those standards may be modified without the need for a variance based upon other considerations determined by the Commission that such considerations would comply with the provisions of the Code and the intent of the PD.
- (4) If used, the owner of the property on which the fence is required to be erected shall permanently and adequately maintain screening fences or walls. The following types of walls or fences shall qualify, Chapter [1141](#) notwithstanding.



## **1156.05 DESIGN PRINCIPLES. (4 OF 6)**

### **(f) Landscaping and Screening. (CONTINUED)**

- (5) Masonry wall or fence. Masonry fences or walls shall be constructed with the finish side out and of any of the following materials:
- A. Native stone
  - B. Brick
  - C. Precast concrete panels with decorative finish or decorative concrete masonry units
  - D. In no case shall more than twenty-five percent (25%) of the area of the fence be erected with common smooth-face masonry units.
- (6) Ribbed metal panel fence. Suitably finished to blend with the primary structure and supported by structurally sound metal frame.
- (7) Vegetative screening. Using plants and fence materials, vegetative screens may be proposed.
- (8) Screening of roof-mounted equipment. All roof-mounted equipment that rises above the roofline of any building or structure

### **(g) Streetscape Improvements.**

- (1) A Streetscape Plan shall be submitted for the entire site. The Streetscape Plan shall address the relationship between vehicular and pedestrian traffic, pedestrian facilities, street and sidewalk lighting, landscaping, street furniture, trash receptacles, and transit stops.
- (2) The design of streets, pedestrian ways, landscaping, lighting, and street furniture shall be coordinated/integrated throughout the site.
- (3) Vehicular streets and driveways shall be designed to be compatible with pedestrian ways to encourage a pedestrian friendly environment. The width of streets shall be sensitive to pedestrian scale, and shall be minimized to avoid overwhelming that pedestrian scale while allowing for efficient vehicular traffic flow.
- (4) Site furnishings such as benches, seating, trash receptacles, bike racks, lighting fixtures, and tree grates shall be addressed in the Streetscape Plan.

### **(h) Service Area and Mechanical Screening.**

- (1) The location of service areas and mechanical equipment shall be considered as part of the overall site design.
- (2) Service areas and mechanical equipment shall be screened from public view.



## 1156.05 DESIGN PRINCIPLES. (5 OF 6)

### (i) Signage.

- (1) A master sign plan shall be prepared illustrating the location, type, size, and materials of all signage, pursuant to Chapter [1329](#) of the Building Code.
- (2) It shall be the duty of the Board of Building Standards/Architectural Board of Review to review the sign proposal. The standards contained in Chapter [1329](#) of the Building Code shall be used as a guide, but those standards may be modified without the need for a variance based upon other considerations determined by the Board of Building Standards/Architectural Board of Review that such considerations would comply with the provisions of the Building Code and the intent of the PD. Generally, the standard will be landscaped monument signs and multi-tenant signs.

### (j) Lighting.

- (1) A lighting plan shall be prepared, including a photometric illustration.
- (2) It shall be the duty of the Board of Building Standards/Architectural Board of Review to review the lighting proposal. The standards contained in Chapter [1306](#) of the Property Maintenance Code shall be used as a guide, but those standards may be modified without the need for a variance based upon other considerations determined by the Board of Building Standards/Architectural Board of Review that such considerations would comply with the provisions of the Building Code and the intent of the PD.
- (3) Lighting shall be designed to avoid spillover onto adjacent properties through the use of cutoff shields or other similar features.

(k) Fences. It shall be the duty of the Architectural Board of Review to review the fence requirements pursuant to Chapter [1153](#) to determine whether said plan is consistent with the provisions of the Code and the intent of the PD.



## 1156.05 DESIGN PRINCIPLES. (6 OF 6)

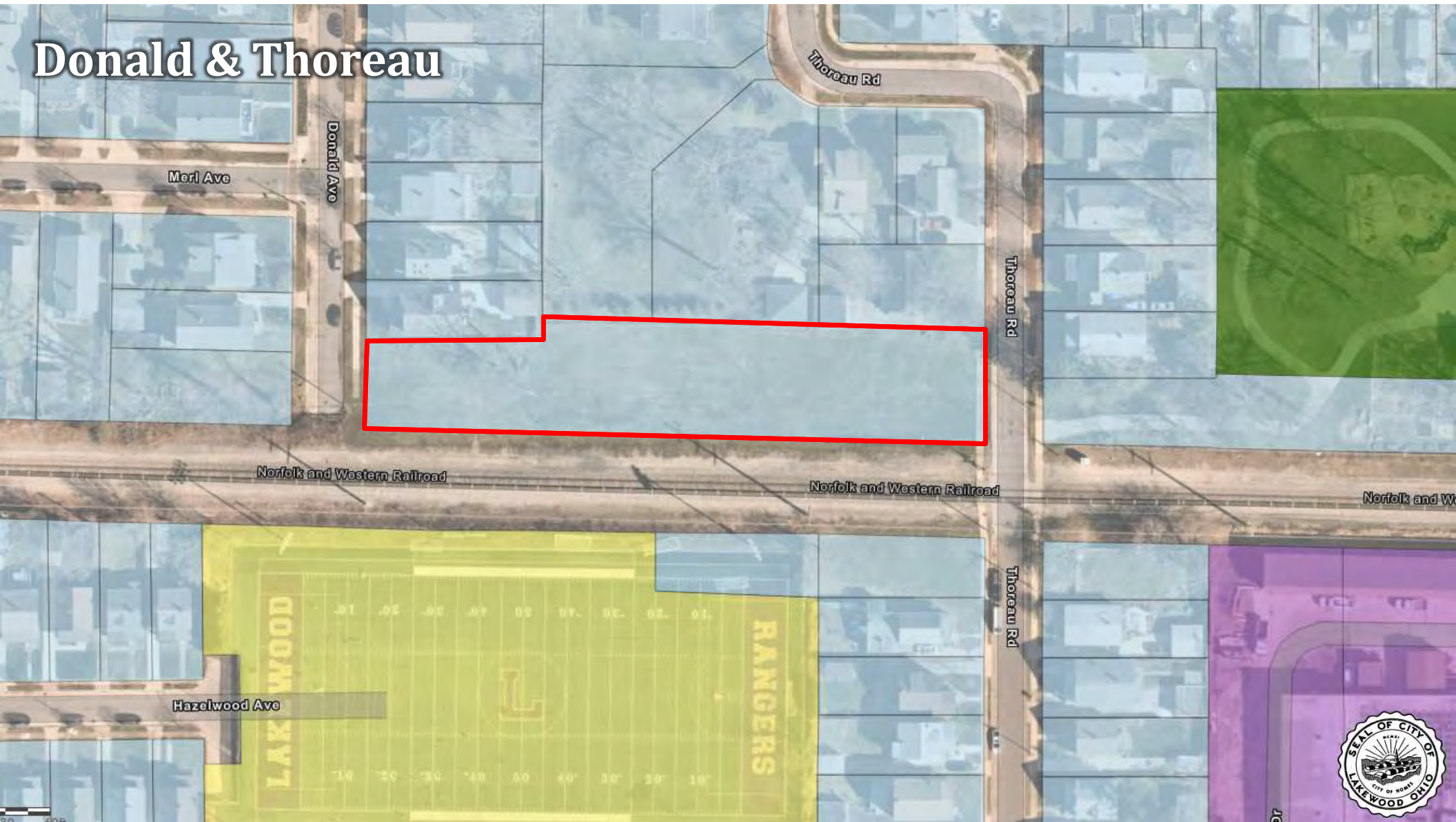
### (l) Urban Open Space.

- (1) Common open space (whether dedicated to public use or owned and maintained in common by the owner or owners) shall be reserved for the leisure and recreational use of all the project's occupants and readily accessible thereto.
- (2) The guideline for PD open space is twenty percent (20%) of the project area.
- (3) Landscaping requirements can be incorporated into the open space requirement pursuant to subsection (f).
- (4) The Commission in making this determination may consider the availability and nature of adjacent or nearby public open space and parkland.
- (5) Common open space is land area of which at least fifty percent (50%) is not covered by buildings, structures or the building's parking spaces.
- (6) Common open space shall be guaranteed by a restrictive covenant in the deed describing the open space and its uses, and requirements regarding maintenance, and improvement that run with the land for the benefit of occupants or the public.

(m) Amenities. All PD's with residential uses should provide on-site amenities within the site which contribute to the open space. These amenities may include but are not limited to: courtyards, a swimming pool, spa, clubhouse, tot lot with play equipment, picnic shelter/barbecue area, court game facilities such as tennis, basketball, or racquetball, or child day care facilities.



# Donald & Thoreau



Merl Ave

Donald Ave

Thoreau Rd

Thoreau Rd

Norfolk and Western Railroad

Norfolk and Western Railroad

Norfolk and Western Railroad

Thoreau Rd

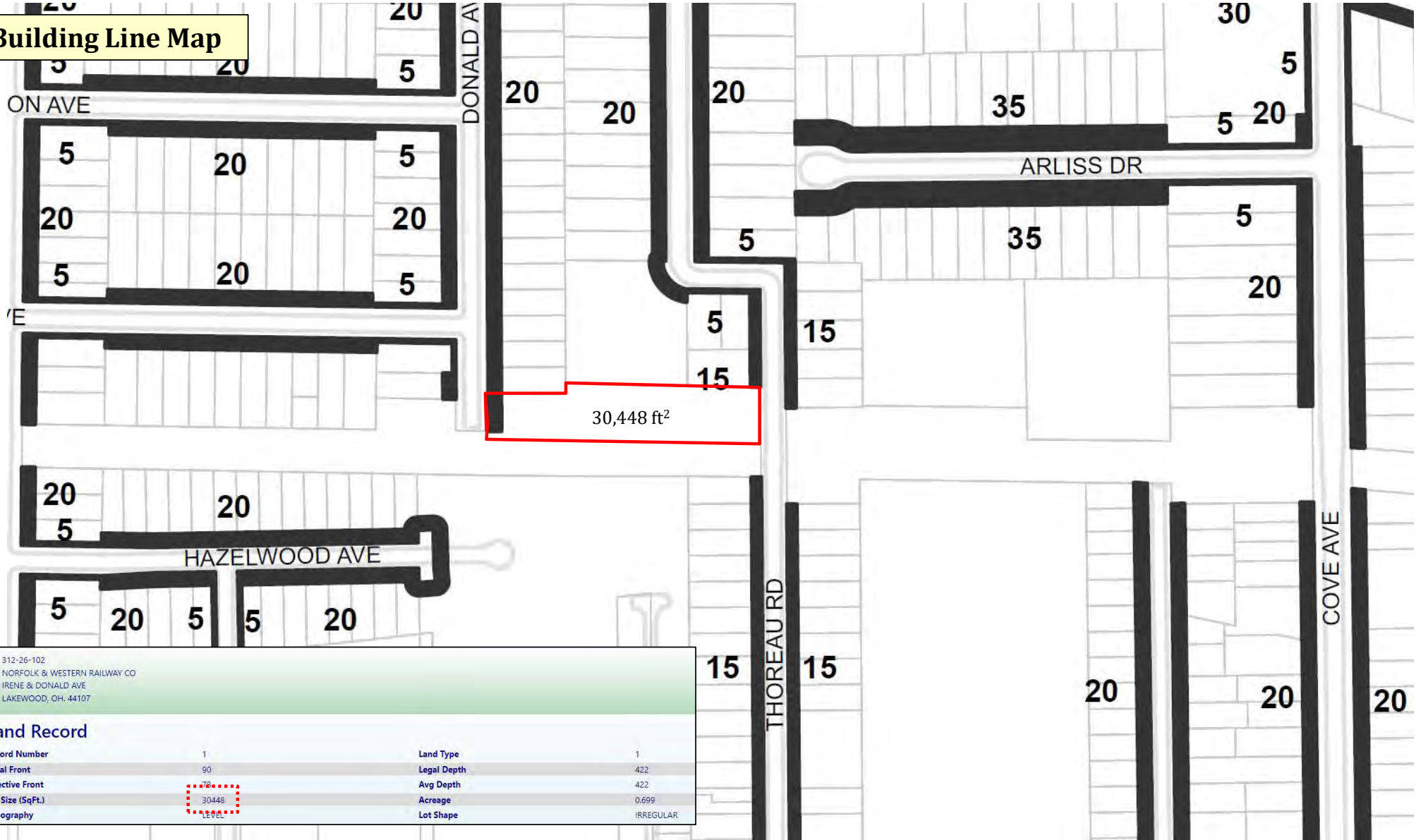
Hazelwood Ave

LAKWOOD

RANGERS



# Building Line Map



312-26-102  
 NORFOLK & WESTERN RAILWAY CO  
 IRENE & DONALD AVE  
 LAKEWOOD, OH, 44107

## Land Record

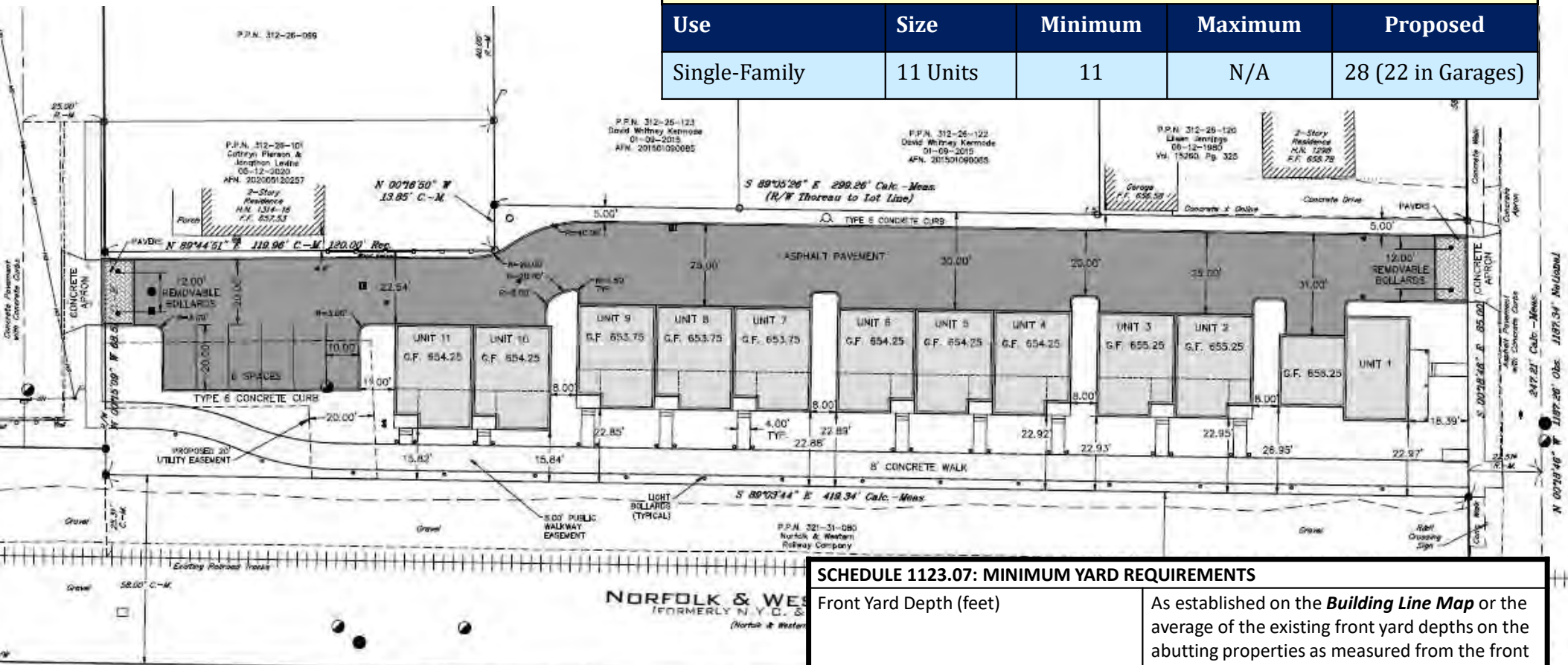
Record Number	1	Land Type	1
Legal Front	90	Legal Depth	422
Effective Front	76	Avg Depth	422
Lot Size (SqFt.)	30448	Acreage	0.699
Topography	LEVEL	Lot Shape	IRREGULAR

## Setbacks + Parking

## 1143.05 SCHEDULE OF USES AND SPACE REQUIREMENTS.

- Residential (Single-Family): Min. 1 per dwelling unit/No Max; 1 required space shall be in a garage

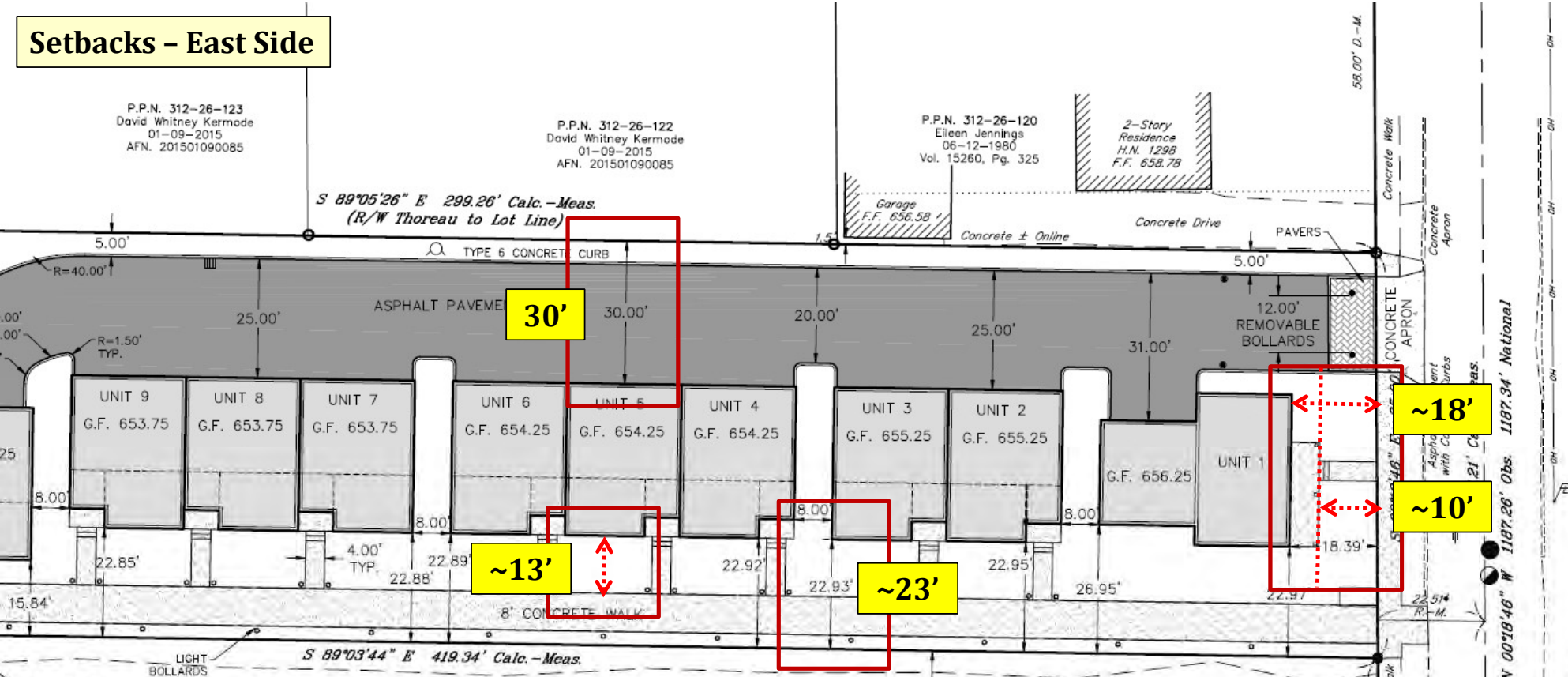
Use	Size	Minimum	Maximum	Proposed
Single-Family	11 Units	11	N/A	28 (22 in Garages)



### SCHEDULE 1123.07: MINIMUM YARD REQUIREMENTS

Front Yard Depth (feet)	As established on the <b>Building Line Map</b> or the average of the existing front yard depths on the abutting properties as measured from the front foundation wall.
Rear Yard Depth (feet)	40
Side Yard Depth (feet)	5
Minimum Sum of Both Side Yards (feet)	15

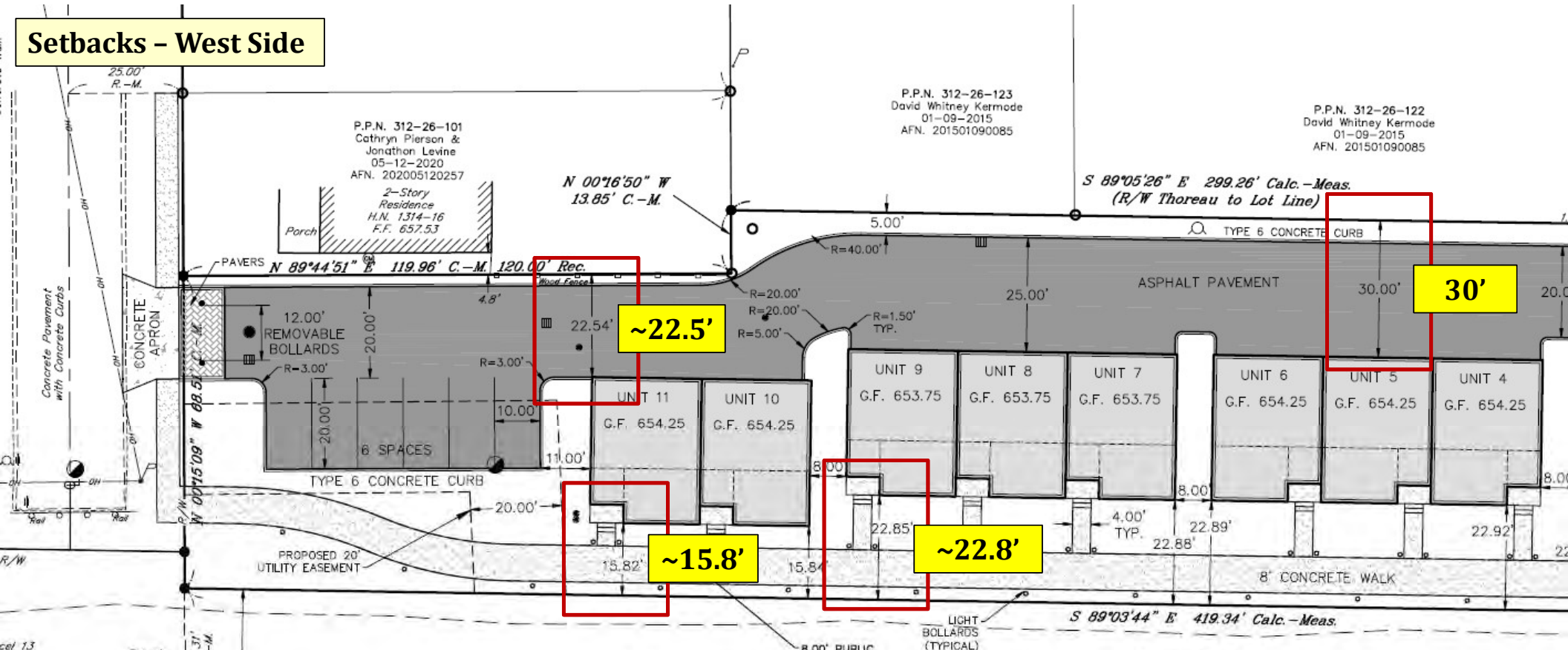
# Setbacks - East Side



**NORFOLK & WESTERN RAILROAD**  
 (FORMERLY N.Y.C. & ST. LOUIS RAILROAD)  
 (Norfolk & Western Parcel A-104)

SCHEDULE 1123.07: MINIMUM YARD REQUIREMENTS	
Front Yard Depth (feet)	As established on the <b>Building Line Map</b> or the average of the existing front yard depths on the abutting properties as measured from the front foundation wall.
Rear Yard Depth (feet)	40
Side Yard Depth (feet)	5
Minimum Sum of Both Side Yards (feet)	15

## Setbacks - West Side

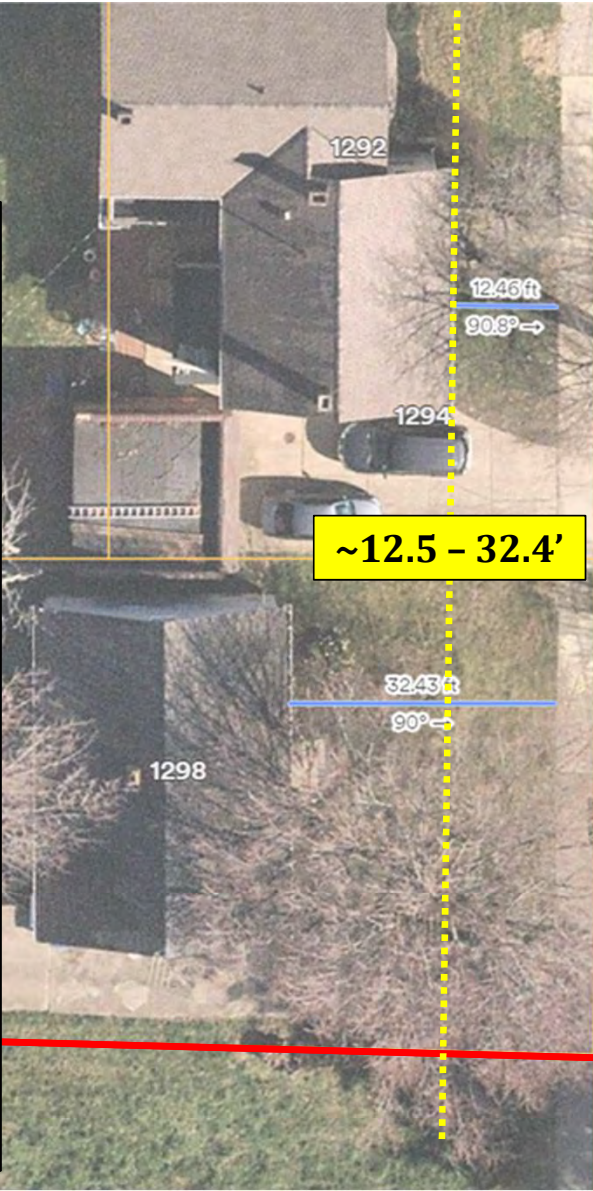
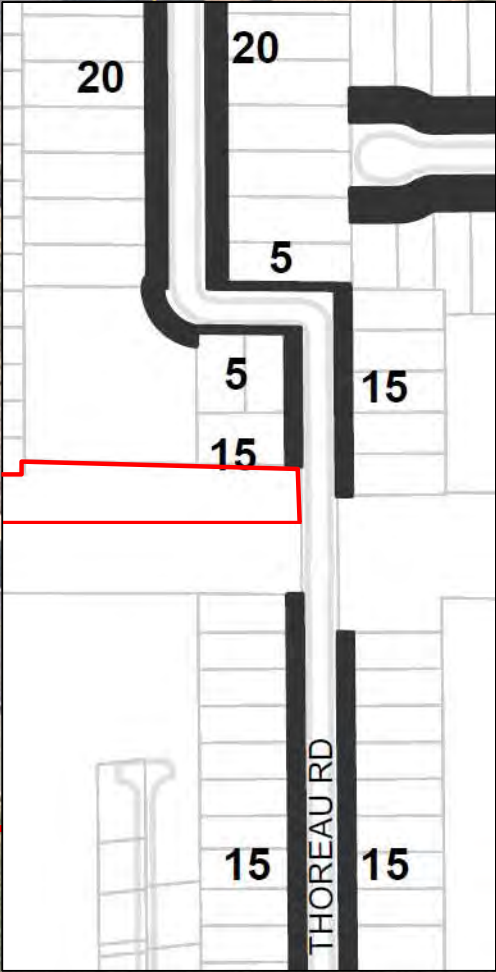


### SCHEDULE 1123.07: MINIMUM YARD REQUIREMENTS

Front Yard Depth (feet)	As established on the <b>Building Line Map</b> or the average of the existing front yard depths on the abutting properties as measured from the front foundation wall.
Rear Yard Depth (feet)	40
Side Yard Depth (feet)	5
Minimum Sum of Both Side Yards (feet)	15

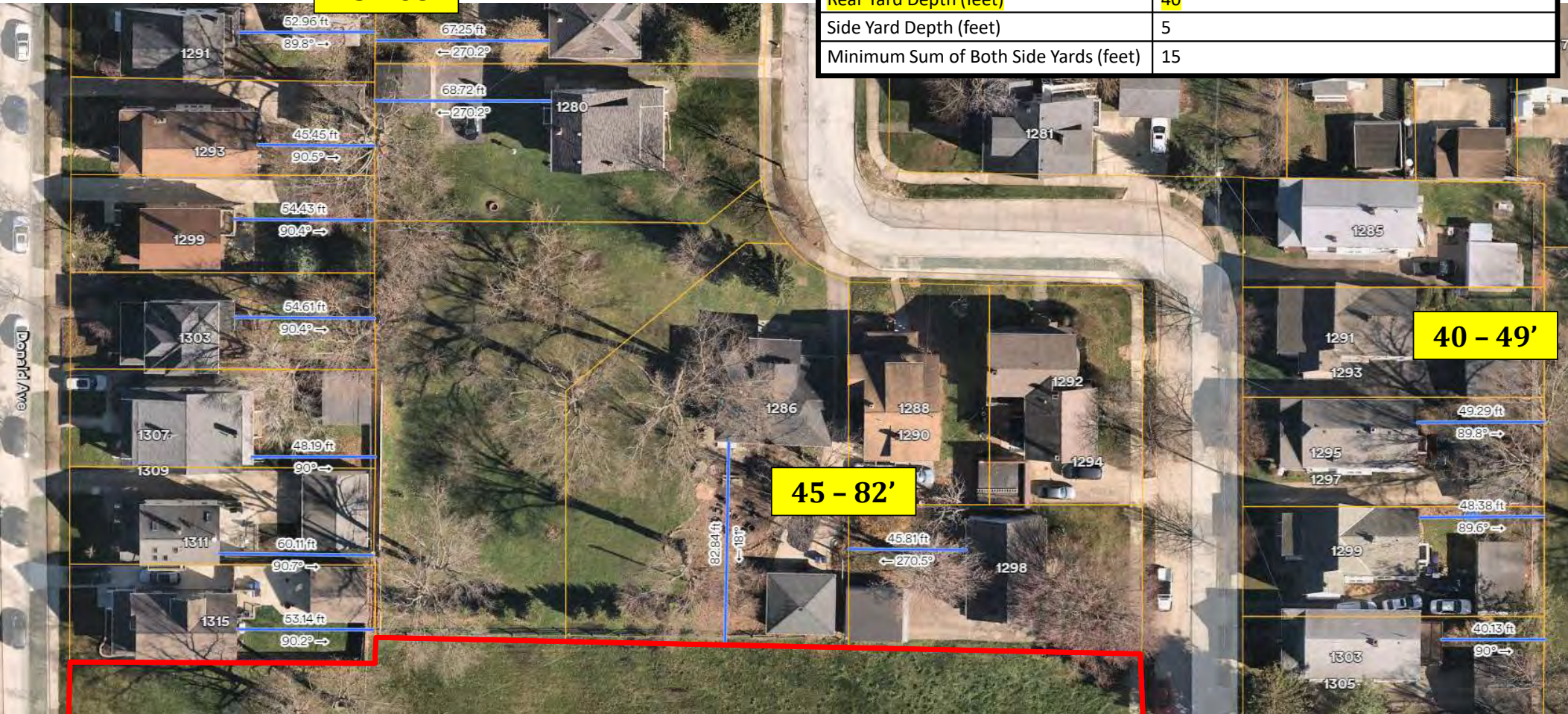
**NORFOLK & WESTERN RAILROAD**  
 (FORMERLY N.Y.C. & ST. LOUIS RAILROAD)  
 (Norfolk & Western Parcel A-104)

**Front Yard Setback  
(Thoreau Comparison)**



## Rear Yard Setback (Area Comparison)

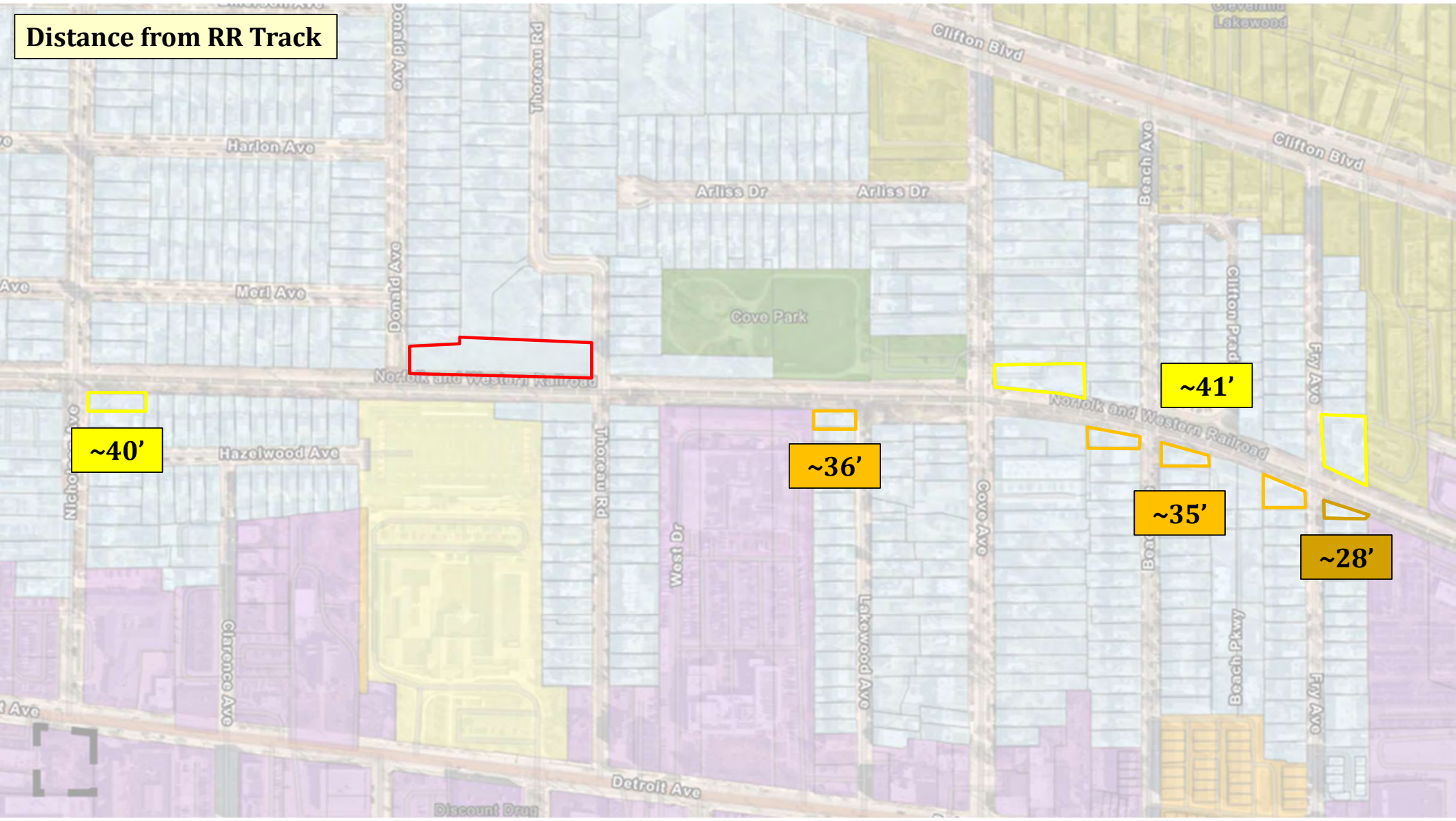
45 - 68'



### SCHEDULE 1123.07: MINIMUM YARD REQUIREMENTS

Front Yard Depth (feet)	As established on the <i>Building Line Map</i> or the average of the existing front yard depths on the abutting properties as measured from the front foundation wall.
Rear Yard Depth (feet)	40
Side Yard Depth (feet)	5
Minimum Sum of Both Side Yards (feet)	15

**Distance from RR Track**



**CHAPTER 1123 - Single- and Two-Family Residential Districts**

**1123.05 HEIGHT REGULATIONS.**

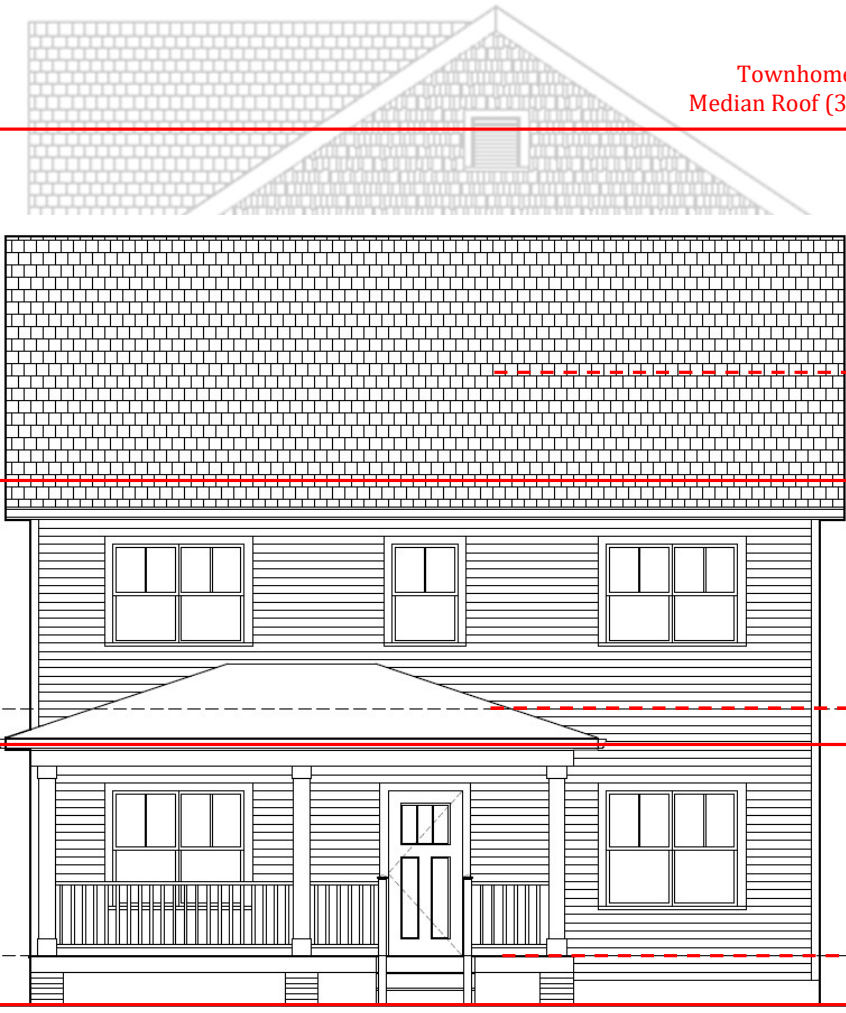
(a) The principal structure shall not exceed thirty-five (35) feet in height above grade as determined by the Commissioner.

**1103.01 GENERAL TERMS; INTERPRETATION.**

(q) BUILDING HEIGHT means the vertical distance measured from the average elevation of the proposed finished grade at the front of the building as determined by the Commissioner to the highest point of the roof for flat roofs, to the deck line of mansard roofs, and the **mean height between the bottom of the eaves and the top of the ridge for gable**, hip and gambrel roofs, exclusive of the building's mechanical systems, chimneys, antennas, or structures necessary to access the roof including but not limited to elevated walkways, stair enclosures and ladders.



ROOF TOP  
 ELEV. 40'-0"  
 ROOF HAIR COCK  
 ELEV. 35'-8"  
 ATTIC FLOOR  
 ELEV. 25'-8"  
 2ND FLOOR  
 ELEV. 12'-0"  
 1ST FLOOR  
 ELEV. 2'-0"  
 FINISH GRADE  
 ELEV. 0'-0"  
 FIRM GRADE  
 ELEV. -1'-0"



Townhome  
Median Roof (35' 8")

Single Family Home  
Median Roof (25' 8")

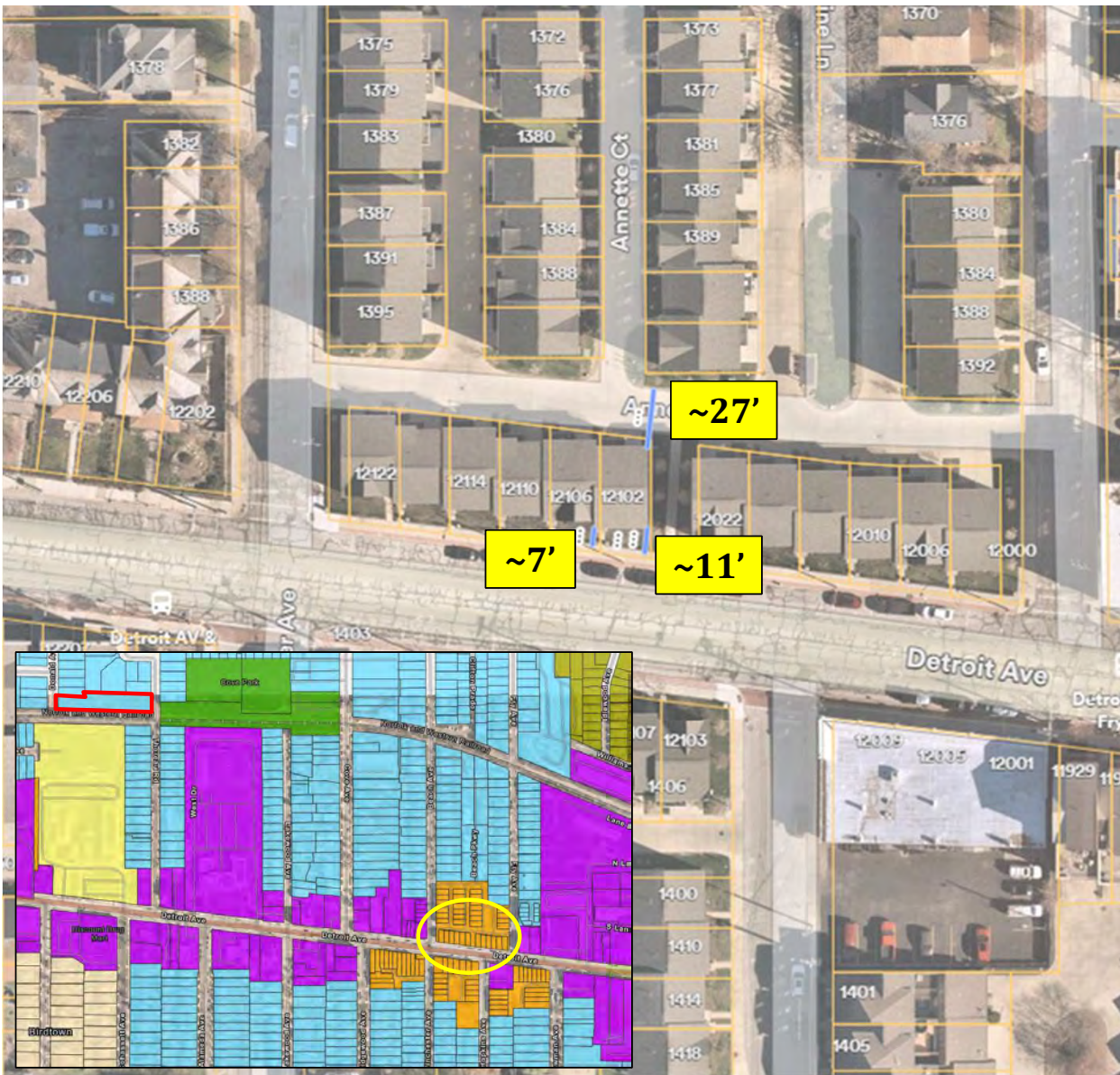
TOP OF ROOF  
 ELEV. 31'-2"  
 MEDIAN ROOF  
 ELEV. 25'-8"  
 ROOF EAVE  
 ELEV. 20'-2"  
 2ND FLOOR  
 ELEV. 12'-0"  
 1ST FLOOR  
 ELEV. 2'-0"  
 GRADE  
 ELEV. 0'-0"

FRONT ELEVATION

1/4" = 1'-0"



# Rockport Townhomes





**McKinley Place Townhomes**





# TMS Engineers, Inc.

Transportation Management Services  
2112 Case Parkway South, #7 • Twinsburg, Ohio 44087  
www.TMSEngineers.com

September 18, 2024

Mr. Adam Conen  
Illiane Realty  
111 S. Peoria Street  
#607  
Chicago, Illinois 60607

**Re: Proposed Residential Development  
Lakewood, Ohio  
Trip Generation Analysis**

TMS Engineers, Inc. has performed the following trip generation analysis for a proposed residential development in the City of Lakewood, Cuyahoga County, Ohio. The development site is located north of the Norfolk & Western Railroad and west of Thoreau Road (See **Location Map, Figure 1**).

The purpose of the trip generation analyses is to estimate the traffic that will be generated by the residential development. The site plan can be seen in **Figure 2**. The following are the results of our trip generation analysis.

### SITE GENERATED TRAFFIC

Calculating future total driveway trips requires an estimate of the traffic generated by the proposed facility. The most widely accepted method of determining the amount of traffic that the proposed development will generate is to compare the proposed land use with existing facilities of the same use. The Institute of Transportation Engineers (ITE) has prepared a document titled "**Trip Generation Manual**", which is a compilation of similar traffic generation studies to aide in making such a comparison. The most recent update of this manual is the 11<sup>th</sup> edition and was utilized for this analysis.

The proposed development is expected to consist of the following land uses:

- 1 Single Family Home (LU #210)
- 10 Town Homes (LU #220)

## Traffic Impact/Trip Generation

Mr. Adam Conen  
September 18, 2024  
Page 2

### PROPOSED TRIP GENERATION CALCULATIONS

Trip generation calculations were performed utilizing data contained in the **Trip Generation Manual, 11<sup>th</sup> Edition** and the methods outlined in the (ITE) **Trip Generation Handbook**. Based on the previously discussed trip generation analysis procedures, the table below shows the estimated site generated traffic during the AM and PM peak hours of the adjacent streets for the proposed residential development. A copy of the trip generation worksheets can be seen in **Appendix A**.

#### NEW TRIP GENERATION

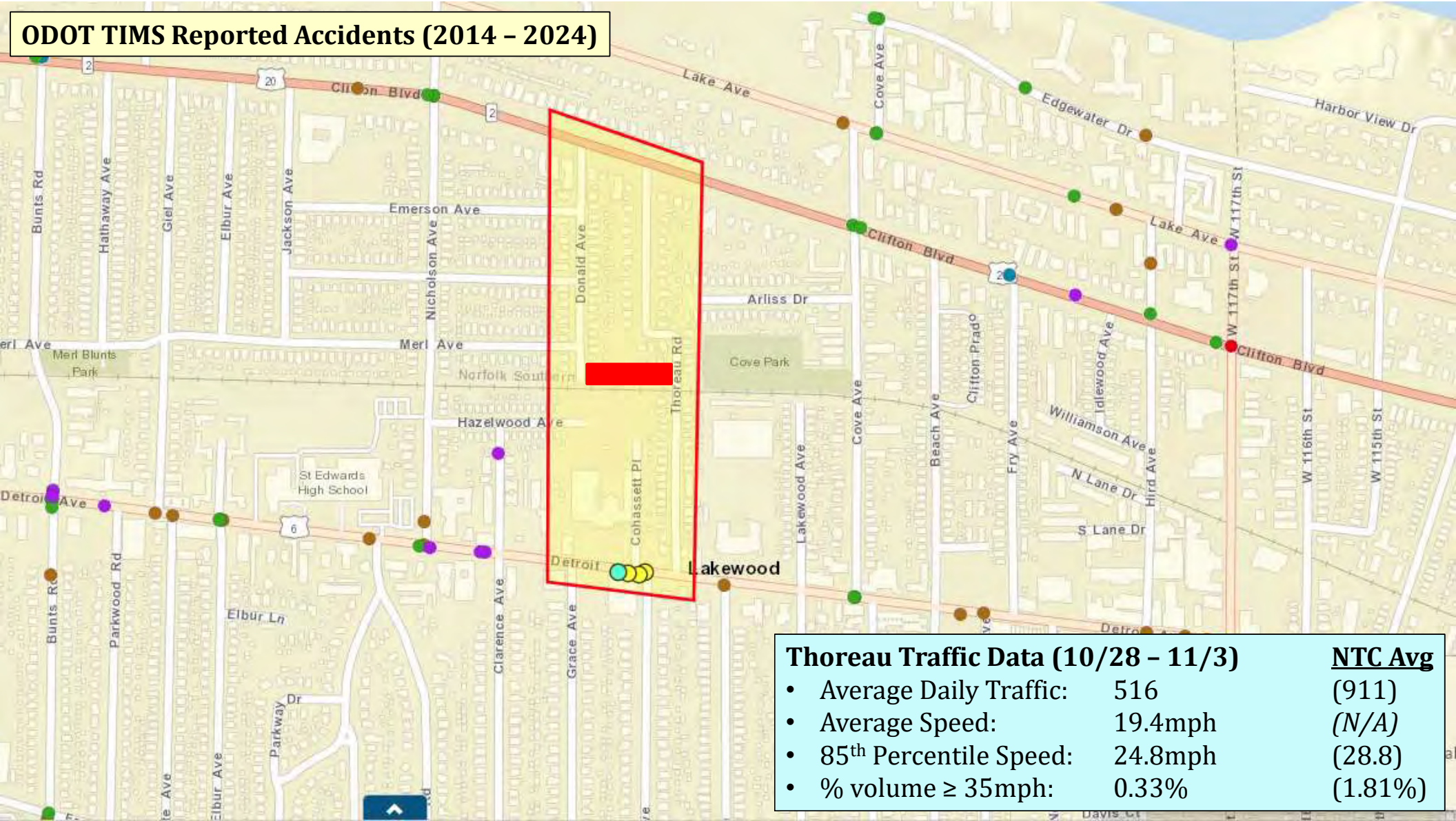
ITE TRIP GENERATION			TRIP ENDS			
ITE CODE	DESCRIPTION	Homes	Weekday AM Peak Hour of Adjacent Streets (Enter/Exit)		Weekday PM Peak Hour of Adjacent Streets (Enter/Exit)	
210	Single-Family Detached Housing	1	0	1	1	0
220	Multifamily Housing (Low-Rise)	10	6	20	16	9
TOTAL NEW TRIPS			27		26	

### CONCLUSIONS

The previous table shows that the proposed residential development is expected to generate a total of 27 trips in the AM peak hour and 26 trips in the PM peak hour. It is our opinion that, when the anticipated changes in traffic volumes are at these levels, the traffic generated by the development should not have an impact on the surrounding street network system.

This opinion is based upon the fact that traffic impact studies are recommended to be performed by the Institute of Transportation Engineers whenever an increase in trips in any peak hour is greater than 100 trips per hour. This recommendation is made because this is the point where a change in roadway capacity may be found and mitigation may or may not be needed. The anticipated generated volumes from this development are less than daily variations in the current volumes on the local roadway network and should not be perceived by the traveling public.

**ODOT TIMS Reported Accidents (2014 - 2024)**

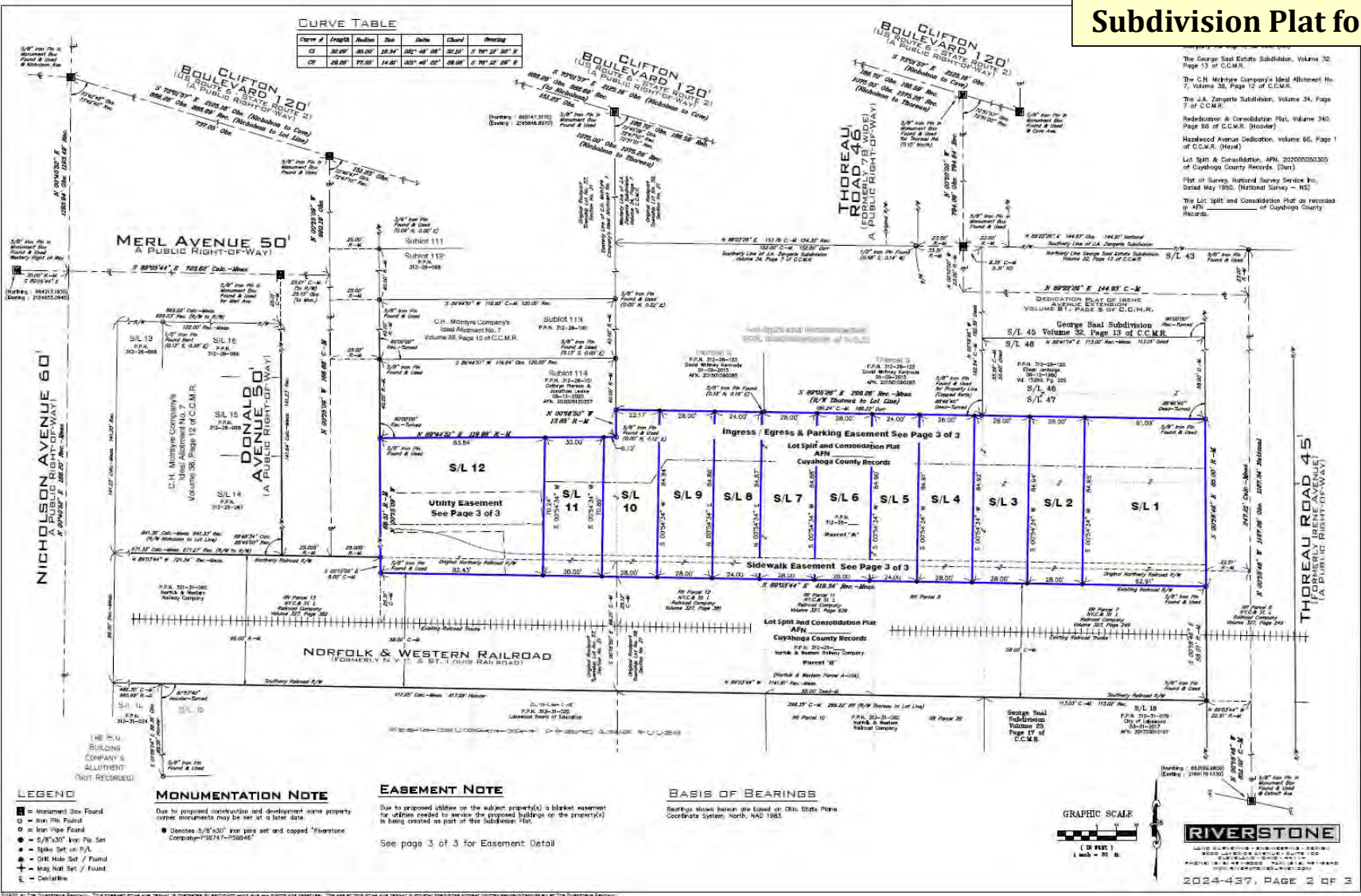


<b>Thoreau Traffic Data (10/28 - 11/3)</b>		<b>NTC Avg</b>
• Average Daily Traffic:	516	(911)
• Average Speed:	19.4mph	(N/A)
• 85 <sup>th</sup> Percentile Speed:	24.8mph	(28.8)
• % volume ≥ 35mph:	0.33%	(1.81%)

# Subdivision Plat for Review

### CURVE TABLE

Curve #	Length	Radius	Delta	Chord	Bearing
C1	38.00'	283.00'	28.34'	102.46'	28.25'
C2	38.00'	283.00'	28.34'	102.46'	28.25'



The George Seal Estate Subdivision, Volume 32, Page 13 of C.C.M.R.  
 The C.H. Michynge Company's Ideal A-100, Volume 30, Page 12 of C.C.M.R.  
 The J.A. Zangerle Subdivision, Volume 34, Page 7 of C.C.M.R.  
 Redeemer's & Consolidation Plat, Volume 340, Page 68 of C.C.M.R. (Hoover)  
 Headwood Avenue Dedication, Volume 66, Page 1 of C.C.M.R. (Hess)  
 Lot Split & Consolidation, APN: 20200050300 of Cuyahoga County Records, (Dart)  
 Plat of Survey, National Survey Service Inc. Dated May 1950, (National Survey - NS)  
 The Lot Split and Consolidation Plat as recorded in APN \_\_\_\_\_ of Cuyahoga County Records.

**LEGEND**  
 ■ = Monument Now Found  
 ○ = Iron Pin Found  
 □ = Iron Pipe Found  
 ● = 6/8" x 30" Iron Pipe Set  
 ● = Spike Set on P/L  
 ● = DRI Hole Set / Found  
 ● = Mag Nail Set / Found  
 L = Corner/Chord

**MONUMENTATION NOTE**  
 Due to proposed construction and development some property corner monuments may be set at a later date.  
 ● denotes 6/8" x 30" iron pipe set and capped "Riverstone Company" P38747-15864E"

**EASEMENT NOTE**  
 Due to proposed utilities on the subject property(s) a blanket easement for utilities needed to service the proposed buildings on the property(s) is being created as part of this Subdivision Plat.  
 See page 3 of 3 for Easement Detail

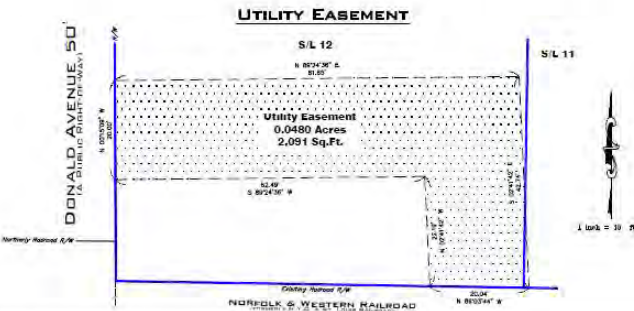
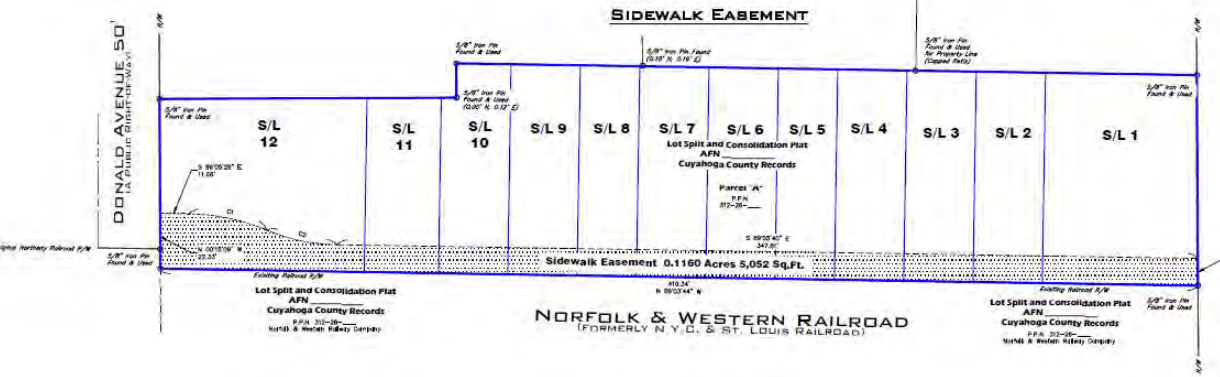
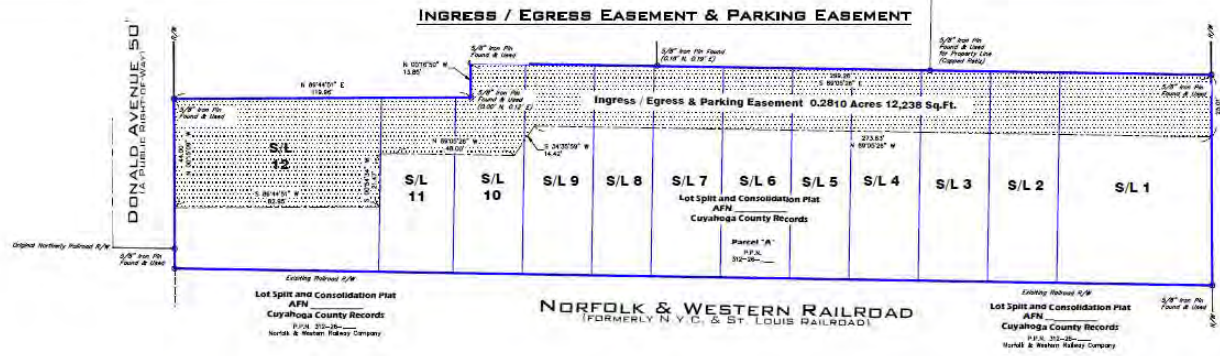
**BASIS OF BEARINGS**  
 Bearings shown herein are based on Ohio State Plane Coordinate System, North, NAD 1983.



**RIVERSTONE**  
 LAND SURVEYING ARCHITECTURE DESIGN  
 10000 W. 100th Street, Suite 100  
 Cleveland, Ohio 44125  
 APN: 20200050300  
 2024-437, PAGE 2 OF 3

DORLAND & VENTURA AND THOREAU AVENUE RD-0084308

# Subdivision Plat for Review



**EASEMENT NOTE**

Due to proposed utilities on the subject property(s) a blanket easement for utilities needed to service the proposed buildings on the property(s) is being created as part of this subdivision Plat.

- LEGEND**
- = Monument Box Found
  - = Iron Pin Found
  - = Iron Pipe Found
  - = 5/8"x20" Iron Pin Set
  - = Spike Set on P/L
  - = DBI Hole Set / Found
  - = May Not Set / Found
  - ⊥ = Centerline

**RIVERSTONE**

LAND SURVEYING & ENGINEERING DESIGN  
 10000 WOODLARK AVENUE SUITE 100  
 CLEVELAND, OHIO 44122  
 (216) 491-1000 FAX (216) 491-1001  
 WWW.RIVERSTONE-SURVEYING.COM

2024-437, PAGE 3 OF 3

DONALD AVENUE AND THOREAU AVENUE RECORDS

**VISION (HOUSING)**...includes a diverse housing stock which meets the varied needs of our community. Affordable and well-maintained housing is maintained by proactive investment in programs and policies that maintain and improve our housing stock. Our Vision develops a culture of informed and responsible action by owner-occupants, landlords, and tenants and provides opportunities for renters to develop community roots.

❑ **Building Connections**

- Has an informed landlord community.
- Has owners who recognize the importance and financial benefits of quality repairs and renovations.
- Benefits from realtors who market Lakewood effectively to prospective homeowners.
- Is home to homeowners who undertake efficiency upgrades such as...installation of new furnaces, adequate insulation, and energy efficient appliances.

❑ **Enhancing Existing Assets**

- Incentivizes investment in and maintenance of existing housing stock.
- Increases awareness among residents about how to better access services provided by local financial institutions.
- Benefits from active and informed block clubs as a conduit for social and informational resources.
- Prioritizes sustainable practices for new construction and the rehabilitation of existing structures.

❑ **Creating Places for People**

- Encourages development of a diverse housing stock that meets the needs of all residents including seniors, low/moderate income families, and special needs households.
- Permits new and infill development which is complementary to the scale and character of surrounding residential uses, integrates sustainable practices, and reflects Universal Design principles.
- Creates and maintains quality greenspace in each neighborhood.

❑ **Striking a Balance**

- Uses point-of-sale inspections as appropriate to ensure safe and well-maintained housing.
- Supports and creates affordable housing for a diversity of incomes and lifestyle needs.
- Conducts proactive code enforcement of residential property including multi-family buildings.

❑ **Thinking about Tomorrow**

- Increases tree canopy, particularly on residential streets.
- Approves quality design which respects the historic character of residential neighborhoods.
- Ensures that abandoned properties are safe, secure, and weatherized.



## **Public Comment Received (October - Summary)**

### **☐ Katherine Herten (1347 Thoreau)**

- Concerns over safety and increased traffic/potential for accidents
- Concerns over developing the existing green space, loss of wildlife habitat

### **☐ Benjamin Kroeck (1237 Thoreau)**

- Requested that housing & road be switched (so road runs along railway)
- Clarification over access road (one-way/two-way?), use of private refuse/snow removal?
- Concerns over increased traffic
- Lack of consideration to existing neighbors through this process.

### **☐ Alexis Kunsak (1237 Thoreau)**

- Traffic, safety/visibility of all pedestrians entering Cove Park
- Traffic study conducted during school year?
- Consideration of speed table?

### **☐ Dave Kermode (1286 Thoreau)**

- Missing elements in application...no detail on individual structures, topographic indications on site plan to access height impact by building.
- Bad faith engagement with adjacent owners.
- Width of northern buffer a significant concern...reduced to 3' with only a 6' fence (no screening of building lighting, impact of road salting on neighboring landscaping)
- PD Code guidelines provides “owners of residential property directly abutting...shall be contacted and offered masonry screening and/or appropriately design alternatives.”

### **☐ David Griffiths (1311 Donald)**

- Property owned by Railroad...agreement between RR and the proposing entity?
- Concern privacy of adjacent homes will be reduced based on height of proposed townhomes.
- Donald Avenue needs to be repaved – badly.

### **☐ Donny & Jackie Davis (1280 Thoreau)**

- Concerns over increase in density, traffic, light and noise pollution
- Introduces housing product not characteristic of the surrounding neighborhood.
- Rear balconies infringe on privacy of neighbors
- No orientation concessions/alterations to proposed plan throughout process.
- Help residents understand more about Hilane Realty

### **☐ Katie Gallagher (1288 Thoreau)**

- Opposed to project.

### **☐ Anna & Kevin Gartland (12593 Clifton Blvd)**

- Project will remove a good deal of green space from neighborhood.
- Increase in population density in area where parking/navigation is often challenging.
- Grave concerns over safety implications – given children frequently ride bikes, play in Cove park, and cross the street near the RR tracks.

### **☐ Cindy Marx (Ward 4 Council Representative)**



## Public Comment Received (November - Summary)

### **Kitty O'Shea (1296 Donald)**

- Concerns over increased parking, traffic issues, snow removal.

### **Debbie Marshall (1281 Thoreau)**

- Concerns over increased traffic, proximity to railroad tracks

### **Abby Tarnai (1267 Thoreau)**

- Sidewalk should be public.
- Drive should be open to both Donald and Thoreau.
- Concerned about grade at railroad crossing & water retention/drainage.

### **Katherine Herten (1347 Thoreau)**

- Concerns over safety and increased traffic/potential for accidents
- Concerns over developing the existing green space, loss of wildlife habitat

### **Benjamin Kroeck (1237 Thoreau)**

- Landscaping/screening inadequate.
- Swap trail and driveway, widen trail to 10'
- Connection to Cove Park?

### **Donny & Jackie Davis (1280 Thoreau)**

- Concerns over increase in density, traffic, light and noise pollution
- Introduces housing product not characteristic of the surrounding neighborhood.
- Reconsider unit count and heights of dwellings.

### **Dave Kermode (1286 Thoreau)**

- 10' landscape buffer with screening to height of balconies.
- Shift units #2 – 9 to south (align w/Units 10/11, increase buffer/landscaping)



### **Applicant Notes (9/18) following meeting with area neighbors.**

**Alternate plan for the project that we studied after meeting with the neighbors...site study that we reviewed internally with our team but had several reasons for not pursuing. Below is our comments on this layout option:**

1. Moving the walkway makes for a difficult connection to the Cove Park path. It dumps onto the sidewalk in the wrong place. The crosswalk cannot align with the paths on either side of Thoreau due to driveway curb cuts to the north and south.
2. Where the driveway meets Donald, it has to curve up to the north to meet the street. This is right where the change in elevation needs to happen. Managing 4' of elevation change along these reverse curves will be difficult.
3. Managing the storm water will be more difficult without the driveway to the north side. Water moves from south to north and also a bit east to west. This is driving the water to the walkway and is pinched at the property line jog.
4. The curb cut for the driveway at Thoreau is really close to the RR gate.
5. Due to the grade differential north to south, likely the front stoops will need more steps (and therefore railings).
6. With the driveway on the south, we are unable to accommodate any decks off the back of the unit above the garage.
7. No opportunity for guest parking with the new driveway layout at Donald.
8. With the walkway to the north, not as easy to dedicate an easement to the city for the walkway. Also, the walkway cannot ever tie into the rail to trail when it happens.
9. With the walkway to the north, it is now more hidden, less visible, and therefore less defensible. This may be a public safety concern.
10. No space for any landscaping between the RR and the units. The driveway takes all the available space.
11. Last, I'm not a fan of having the front doors facing the adjacent backyards. Refer to #9.

**Submitted presentation has made changes to address some of the neighbors' concerns. We did:**

- Move the units farther from the property line
- Made revisions to the rear elevation of the units
- Extended some landscaping back and between the units
- Adjusted the parking to add more spaces; and
- Made revisions to the entry of the drive with pavers and removeable bollards.

**They were not concerned with adding more landscaping to the north property line. With the fence along their property, they did not feel any additional landscaping was needed on their behalf.**



### 1156.03 STANDARDS FOR REVIEW OF A PRELIMINARY PD PLAN.

An application for approval of a Preliminary PD Plan, together with submitted plans and reports, shall be reviewed for its conformance with the following standards:

- (a) ...sensitive to the abutting land uses and to the zoning designation which the PD is replacing. **Mixed use within the planned development or within the same structure located in the PD is encouraged** where appropriate;
- (b) ...comply with the subdivision requirements as set forth in Chapter 1155 of this Code, except to the extent modifications, variances, or waivers have been expressly allowed pursuant to paragraph (e) below;
- (c) **Adverse impacts on adjacent properties**, including but not limited to increased traffic or noise, as described in Chapter 515 of the Ordinances, and visual impacts, shall be mitigated to the maximum extent feasible;
- (d) ...**integrated with adjacent development through street connections, sidewalks, trails**, and similar features;
- (e) All district, development, and subdivision standards...may be modified or varied upon a finding that the proposed PD **incorporates creative site design which represents an improvement in quality and service of the purposes set forth in Section 1156.01...** including but not limited to improvements in open space provision and access; environmental protection; tree/vegetation preservation; efficient provision of streets, roads, and other utilities and services; or choice of living and housing environments;
- (f) ...**shall comply with the following requirements**, which shall not be modified or varied except as expressly set forth below **or as permitted by the Commission.**
  - (1) Minimum area requirement. All Planned Developments shall have a **minimum size of 10,000 square feet.**
  - (2) Setbacks from adjoining residential uses. A Planned Development shall comply with any applicable zone district standards that require **minimum setbacks from adjoining residential uses or properties** as set forth in Chapter 1121 and 1123.
  - (3) Environmental protection standards. All Planned Developments shall comply with Ohio Environmental Protection Agency regulations and local ordinances.
  - (4) Architectural and design standards. All Planned Developments shall comply with all architectural and design standards, including those set forth in Section 1156.05, Design Principles, of this Code and Chapter 1325 of the Building Code.

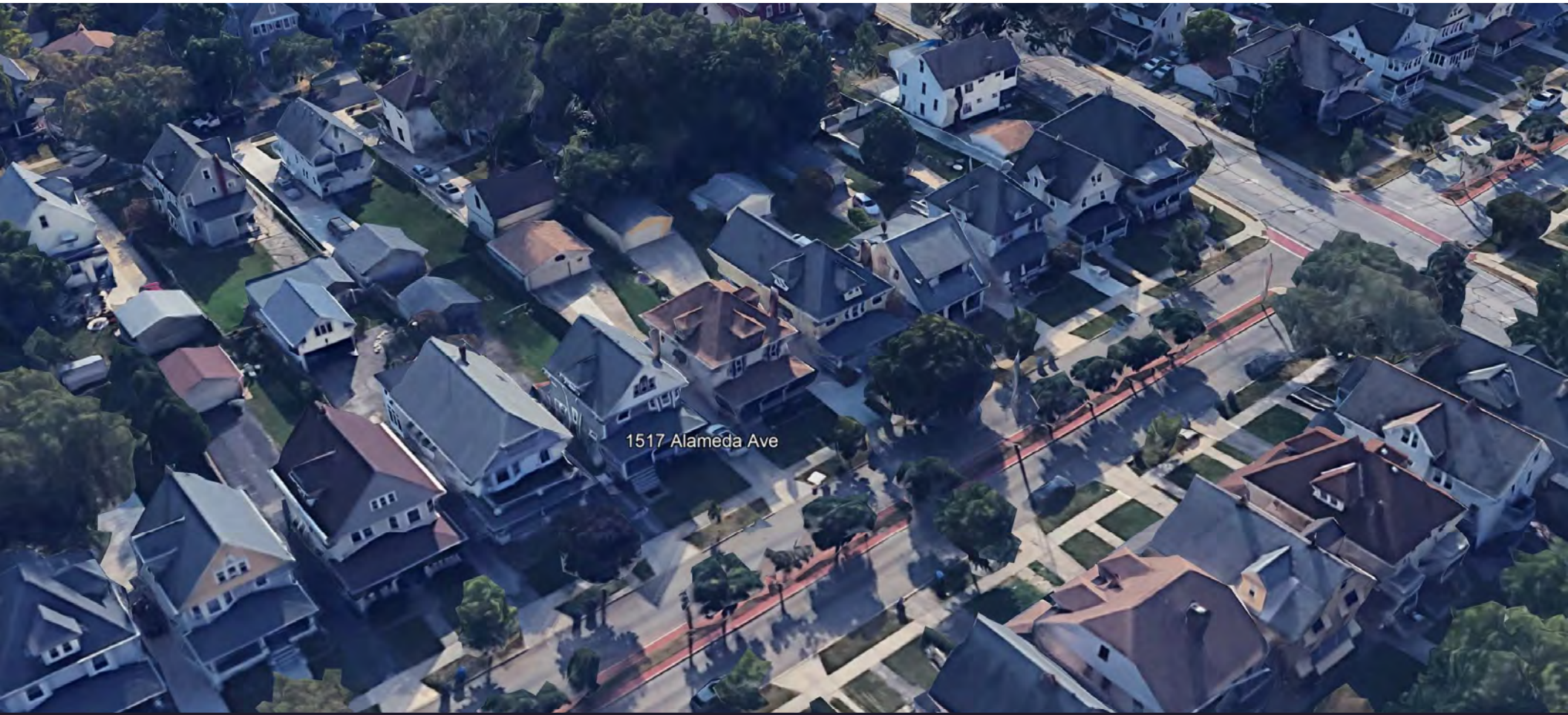


# Recommendation (10-37-24) – Planned Development (Final)

Review for final approval

*Points to be addressed (from October meeting) were:*

- *The buffer.*
- *Provide a plat, at least a working one.*
- *Placing the eight-foot-wide path in a public easement for public use as a sidewalk.*
- *Provide more information on tree replacement ratio.*
- *Provide a full-blown landscape plan with samples.*
- *One-way directional flow of the driveway, from Thoreau Avenue to Donald Avenue, with signage and potentially **bollards**.*



1517 Alameda Ave



# **Docket 11-39-24 – Parking Plan Review**

## **1517 Alameda (Exception to Required Minimum)**

## **Request (11-39-24)**

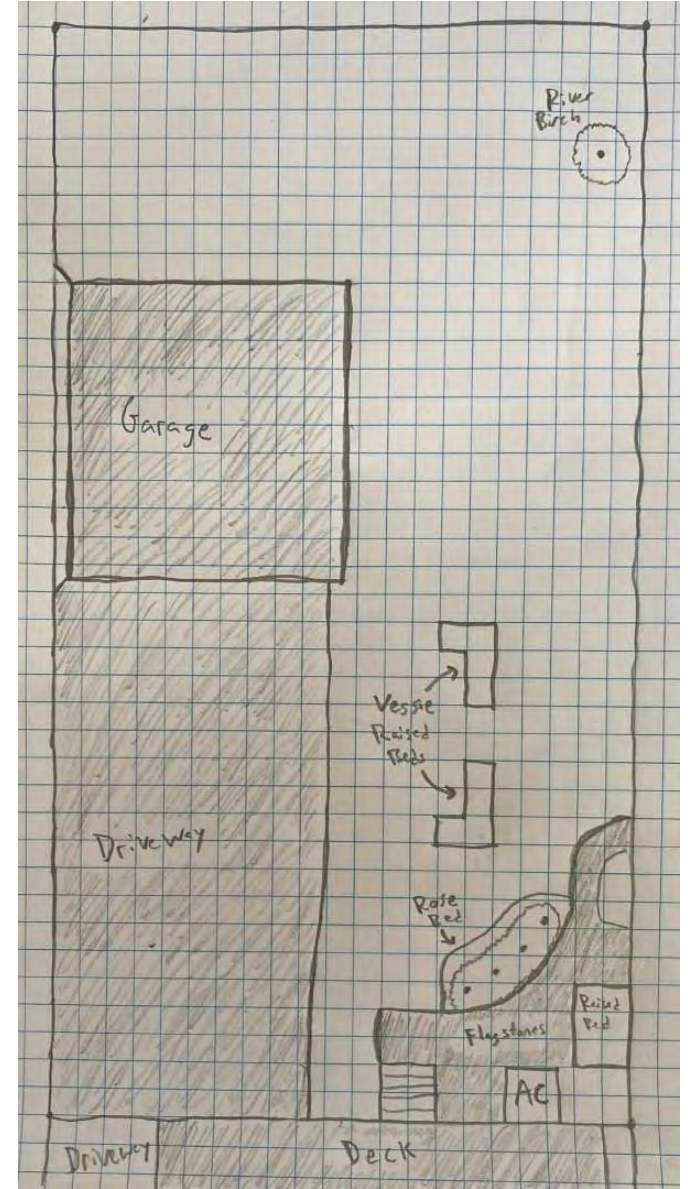
To review and approve a **Parking Plan (Exception to Required Minimum)** pursuant to:

- Chapter 1143 - Parking.

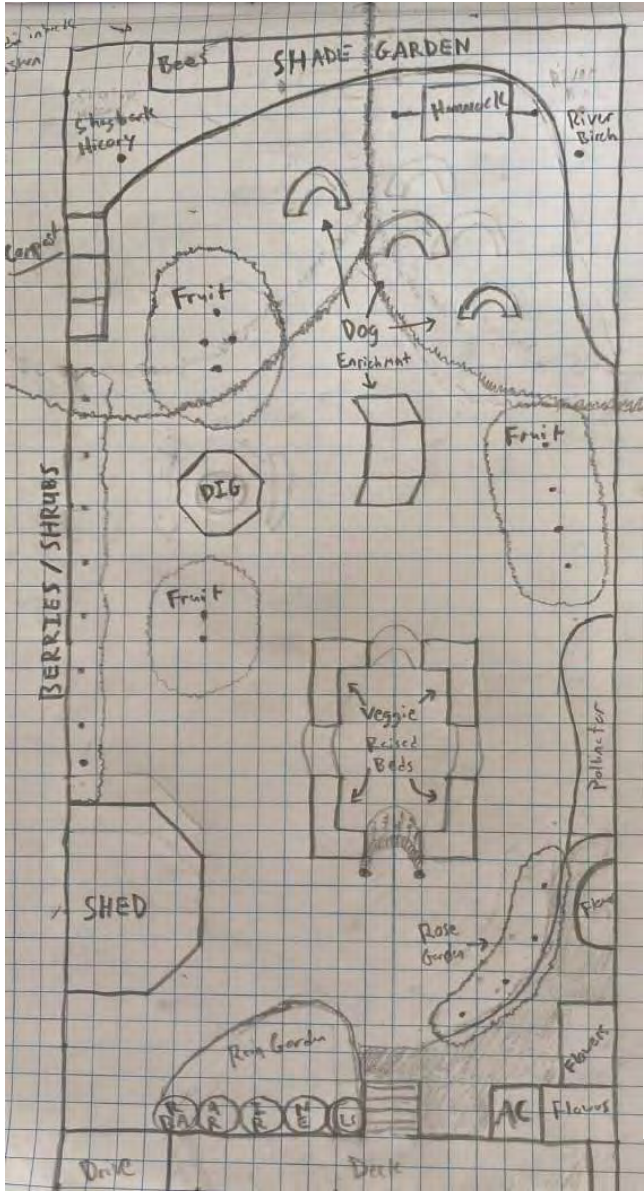
Proposal is in the R2 - Residential, Single and Two Family, District.

1517 Alameda Ave









**Lakewood City Code**

For uses defined as Residential in Section 1143.04, the Commission may consider the applicant's implementation of an improvement to the property that aligns with the general goals and objectives as described in the Vision including, but not limited to, the following:

- (1) Improvements to the property that may reduce carbon emissions and improve energy efficiency using environmental best management practices (BMPs) promulgated by the Environmental Protection Agency;
- (2) Storm water management techniques such as bio-swaales, rain gardens, and pervious pavements;
- (3) A shed or storage building on site for landscaping equipment and additional storage;
- (4) An innovative landscaping plan, considered to be over and above the landscaping requirements as typically required by the Architectural Board of Review, which may include plant, flower and tree type, size, design, location and irrigation;

**Incorporation into**

1. The "Your Yard and Clean Air" article within [Ohio EPA's Practice Greenscaping](#) webpage provide a range of options to reduce air pollution from gas-powered landscaping tools, including "Use low-maintenance... grass/flower seed mixtures that grow slowly and require less mowing" and "Decrease lawn area. Plant additional trees and shrubs to reduce the energy costs of heating and cooling your house and to provide landscaping for wildlife. Native wildflowers and plants require little to no maintenance after planting." We are planning to plant

U.S. ENVIRONMENTAL PROTECTION AGENCY  
OFFICE OF MOBILE SOURCES

### Your Yard and Clean Air

**Small Engines are Big Polluters**  
Most people do not associate air pollution with mowing the lawn. Yet emissions from lawn mowers, snow blowers, chain saws, leaf vacuums, and similar outdoor power equipment are a significant source of pollution. Today's small engines emit high levels of carbon monoxide, a colorless, odorless, poisonous gas. They also emit hydrocarbons and nitrogen oxides, pollutants that contribute to the formation of ozone. While ozone occurs naturally in the upper atmosphere and shields the earth from harmful radiation, ozone at ground level is a noxious pollutant. Ground-level ozone impairs lung function, inhibits plant growth, and is a key ingredient of smog.

Emission control for small gasoline engines has not been a crucial design consideration until now. Consequently, small engines are big polluters. And power equipment users inadvertently contribute to the problem by carelessly handling fuel and by improperly maintaining their equipment.

The U.S. Environmental Protection Agency (EPA) and the power equipment industry are working to investigate and bring to market cleaner technology for small engines.

**Pollution Prevention in Your Own Backyard**  
EPA anticipates that regulations now being developed will bring cleaner lawn and garden equipment to market within a few years. Meanwhile, consumers can make a difference by adopting practices that will help protect the environment now and in the future.

**Avoid spilling gasoline.**  
Preventing spills and overfills is an easy and effective way for power equipment owners to prevent pollution. Even small gasoline spills evaporate and pollute the air.

Use a gasoline container you can handle easily and hold securely. Pour slowly and smoothly. Use a funnel, or a spout with an automatic stop device to prevent overflowing the gas tank. Keep the cap or spout and the vent hole on gasoline containers closed tightly. Transport and store gasoline and power equipment out of direct sunlight in a cool, dry place. Use caution when pumping gasoline into a container at the gas station.

FACT SHEET OMS-19  
May 1999

Practice Green Landscaping

**Your Yard and Clean Air.** This document provides tips on how you can prevent pollution in your own backyard by adopting practices that will help protect the environment now and in the future.

**Green Landscaping with Native Plants.** This site provides a wizard that answers commonly asked questions about landscaping with native wildflowers and grasses in the Great Lakes region.

**Greenscaping.** Reduce the environmental impacts of landscaping your lawn and property. See the [U.S. EPA Greenscaping Your Lawn and Garden Guide](#) or follow these quick tips:

- Keep your yard healthy—detritus, use mulch, etc.
- Sweep outside instead of using a hose.
- Landscape using rain garden techniques to save water and reduce stormwater runoff.
- Minimize the need for pesticides by choosing plant species that are resistant to insects and disease.

Landscaping with native plant species works best. Provide habitats for bees, bats, toads, etc. that prey on insect pests. Introduce praying mantises, lacewings, ladybugs and other pest-eaters to your garden.

Using Green Roofs to Reduce Heat Islands

**Heat Islands**

**Green Roofs Resources**

- Types of Green Roofs
- Green Roofs in Cold Climates
- Co-Benefits of Green Roofs
- Costs of Installing and Maintaining Green Roofs
- More Information

Green roofs have been proven to help reduce heat islands, improve performance over other materials. This page provides a brief overview of the role of green roofs in mitigating the heat island effect, including green roof types, other environmental and social benefits, and cost-benefit considerations.

A green roof or rooftop garden is a vegetation layer grown on a building, terrace or ground level. Green roofs reduce heat from the air and reduce temperatures of the roof surface and surrounding air, using green roofs in cities or other built environments can benefit vegetation can moderate the heat island effect, particularly during the day. Summer roof temperatures can be 30-40°F lower than those of conventional roofs and can reduce city-wide ambient temperatures by as much as 1°F. In addition, green roofs can reduce building energy use by 5-7% compared to conventional roofs, reducing peak electricity demand and leading to an annual savings of 10-20 per cent of the cost of the roof's surface. These temperature reduction and energy

## **1143.09 PARKING PLAN REVIEW: PLANNING COMMISSION.**

(a) The Commission shall review applications for parking plans that do not meet the set schedule in [1143.05](#). In addition to Section [1143.09](#) and Section [1143.10](#), the Commission may consider the following when reviewing an application:

- (1) Impact on central character of residential neighborhoods taking on overflow parking;
- (2) Available surface parking lots in the neighborhood that could be used for shared parking;
- (3) Similarly scaled projects throughout the City to compare parking footprint;
- (4) When a restaurant use is proposed, the total number of tables to parking spaces:
- (5) Total number of employees:
- (6) Alternative forms of transportation available in the neighborhood;
- (7) Implementation of bicycle facilities, including but not limited to, bicycle racks, covered bicycle parking, and shower facilities for employees;
- (8) Peak demand for parking spaces from all uses compared to the total supply of spaces:
- (9) Traffic impact analysis and/or a traffic demand study;
- (10) For uses defined as Institutional or Public Assembly in Schedule [1143.05](#), the following guidelines:
  - A. One space for each 80 square feet of all auditoria and public assembly rooms: and
  - B. One space for each employee.



## 1143.10 EXCEPTIONS TO REQUIRED MINIMUMS.

The number of parking spaces required may be reduced in accordance with the following credits as determined by the Commission when reviewing an application for a reduction to the minimum number of parking spaces required per Schedule [1143.05](#).

(a) For uses defined as Commercial...

(b) For uses defined as Residential in Section [1143.04](#), the Commission may consider the applicant's implementation of an improvement to the property that aligns with the general goals and objectives as described in the Vision including, but not limited to, the following:

- (1) **Improvements to the property that may reduce carbon emissions and improve energy efficiency using environmental best management practices (BMPs) promulgated by the Environmental Protection Agency;**
- (2) **Storm water management techniques such as bio-swales, rain gardens, and pervious pavements;**
- (3) **A shed or storage building on site for landscaping equipment and additional storage;**
- (4) **An innovative landscaping plan**, considered to be over and above the landscaping requirements as typically required by the Architectural Board of Review, which may include plant, flower and tree type, size, design, location and irrigation;
- (5) For properties not connected properly to the City's sanitary and storm system per Section [913.05](#), necessary improvements made by the applicant to connect correctly.


(Ord. 36-14. Passed 1-20-2015.)

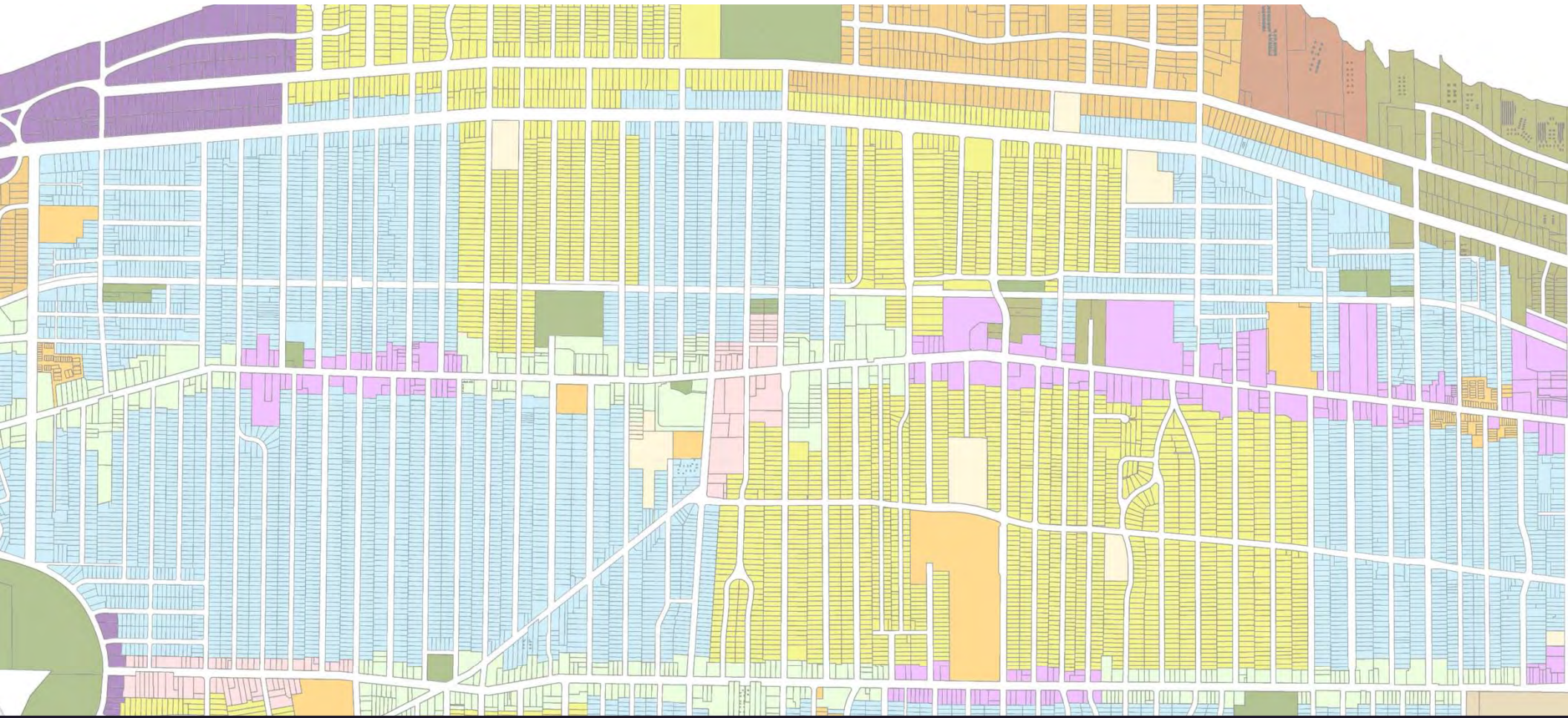


## **Recommendation (11-39-24) – Parking Plan**

Recommend approval

1517 Alameda Ave





**Docket 11-40-24 – ODOT Excess Land Transfer (Glenbury Dedication)**  
Communication for Review/Ordinance 18-2024



## **Request (11-40-24)**

The review of **Ordinance 18-2024 (Glenbury Dedication)**.

- Dedicate a small portion of Glenbury Avenue as public right-of-way.

**Provide Recommendations, Recieve & File**





**LAW DEPARTMENT  
OFFICE OF PROSECUTION**  
12650 Detroit Avenue | Lakewood, Ohio 44107  
(216) 529-6030 | Fax (216) 228-2514  
[www.lakewoodoh.gov](http://www.lakewoodoh.gov)  
[ernest.vargo@lakewoodoh.gov](mailto:ernest.vargo@lakewoodoh.gov)  
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JENNIFER L. SWALLOW  
CHIEF ASSISTANT  
LAW DIRECTOR

ANDREW N. FLECK  
ASSISTANT PROSECUTOR/  
ASSISTANT LAW DIRECTOR

October 7, 2024

Lakewood City Council  
Lakewood, Ohio

Re: ODOT Excess Land Transfer

Dear President Kepple and Members of City Council,

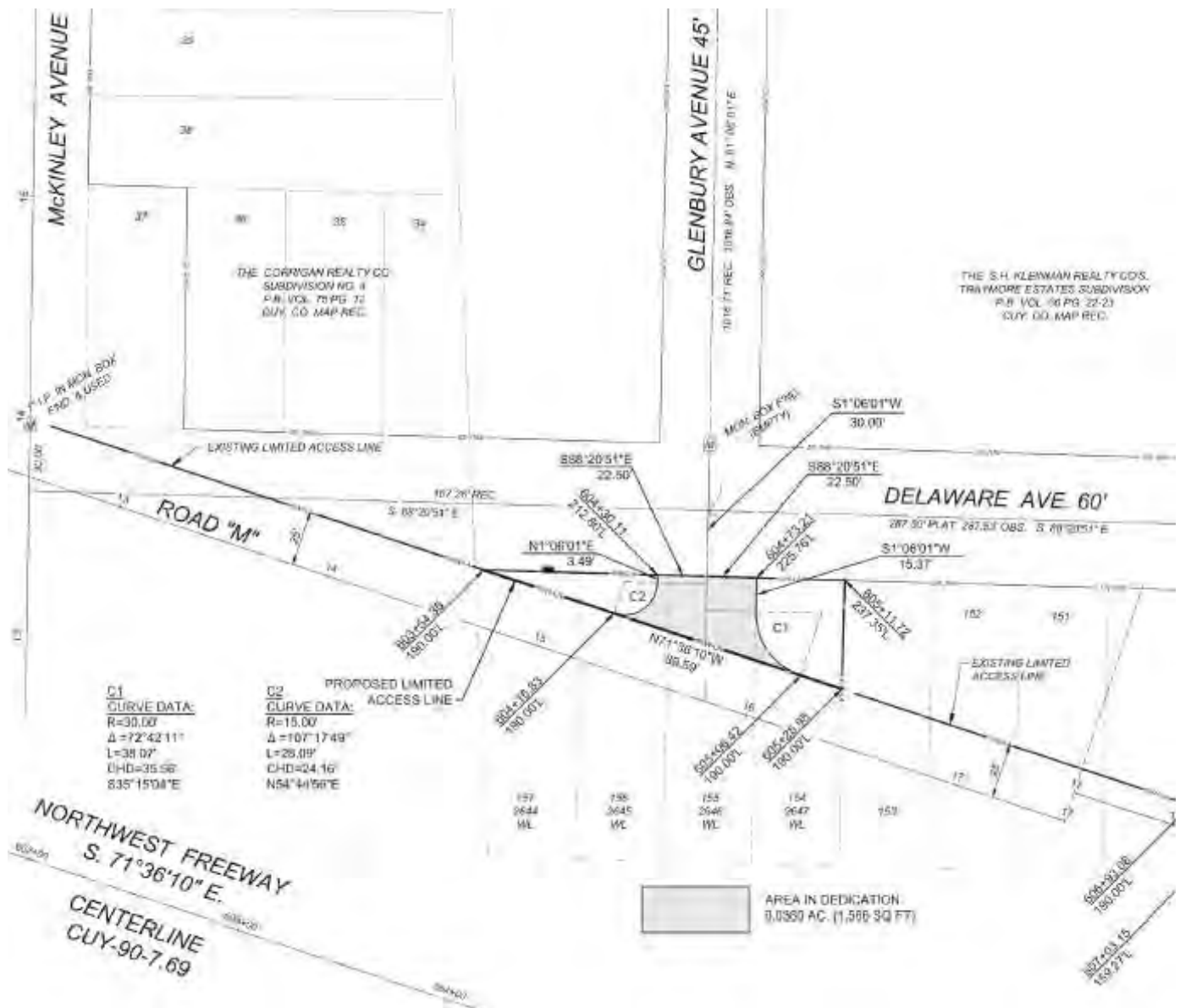
Attached please find two pieces of legislation, which if adopted will vacate a small portion of Niagara Drive from the public right-of-way and dedicate a small portion of Glenbury Avenue as public right-of-way. The legislation is necessary because the City long ago ceased to use the Niagara Drive parcel as a public street and at some time began use of the Glenbury Avenue parcel as a public street.

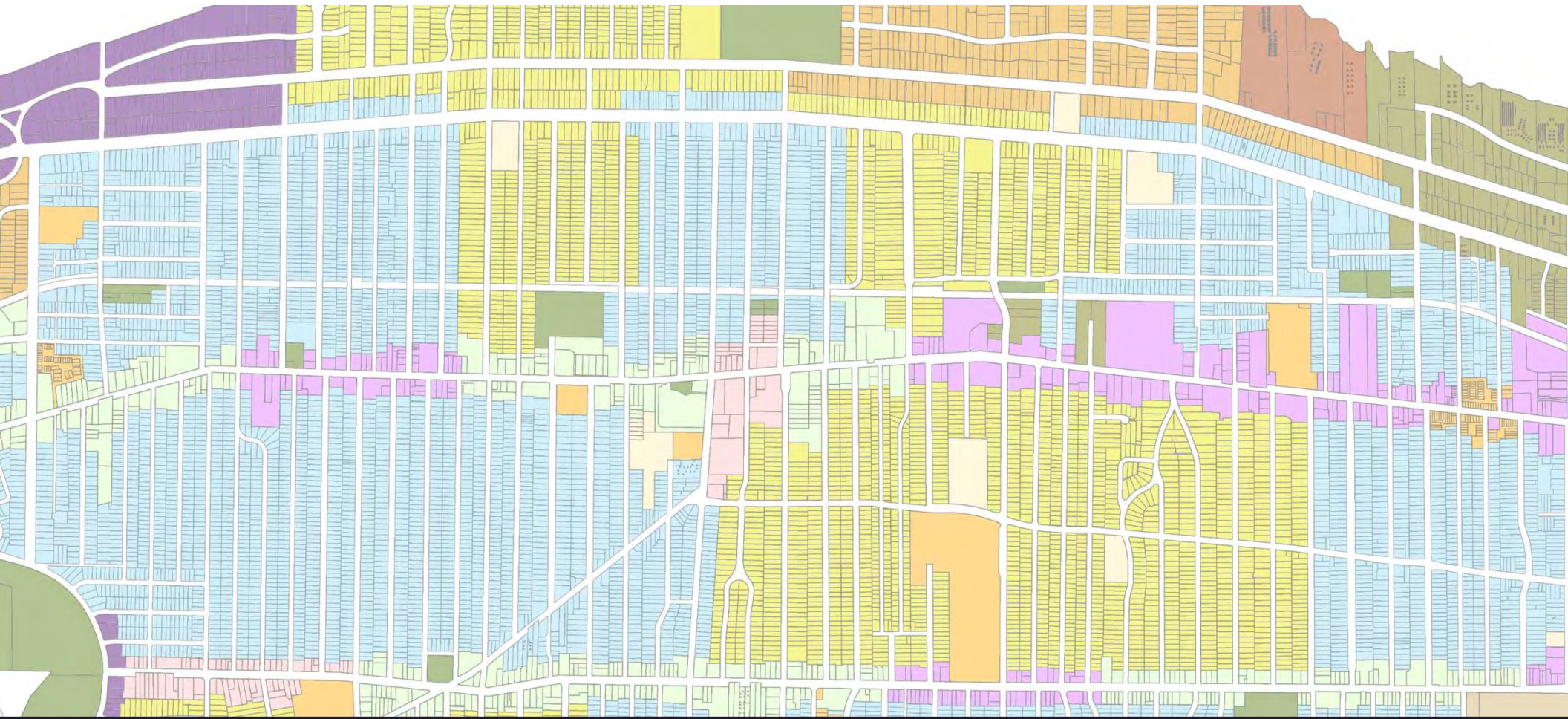
Over the past months, the City has worked closely with the Ohio Department of Transportation (“ODOT”) to effect the transfer to the City of small parcels of property along North Marginal and Interstate 90. A recent survey of the property revealed that a small portion of Niagara Drive was never formally vacated as a public right-of-way during the construction of Interstate 90 despite the fact that it has no longer been used as a public street since the construction. At some time since the construction of Interstate 90, moreover, the City did begin use of the Glenbury Drive parcel as a public street without its formal dedication as such.

This legislation is an important step in facilitating ODOT’s transfer of the parcels to the City. Thank you.

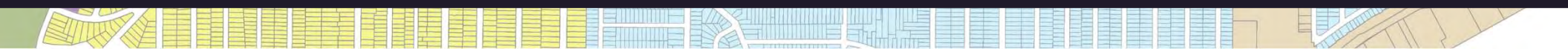
Respectfully,  
*Ernest E. Vargo*  
Ernest E. Vargo







**Docket 11-41-24 – ODOT Excess Land Transfer (Niagara Vacation)  
Communication for Review/Ordinance 17-2024**



## **Request (11-41-24)**

The review of **Ordinance 17-2024 (Niagara Vacation)**.

- Vacate a small portion of Niagara Avenue from public right-of-way.

**Provide Recommendations, Recieve & File**





**LAW DEPARTMENT  
OFFICE OF PROSECUTION**  
12650 Detroit Avenue | Lakewood, Ohio 44107  
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October 7, 2024

Lakewood City Council  
Lakewood, Ohio

Re: ODOT Excess Land Transfer

Dear President Kepple and Members of City Council,

Attached please find two pieces of legislation, which if adopted will vacate a small portion of Niagara Drive from the public right-of-way and dedicate a small portion of Glenbury Avenue as public right-of-way. The legislation is necessary because the City long ago ceased to use the Niagara Drive parcel as a public street and at some time began use of the Glenbury Avenue parcel as a public street.

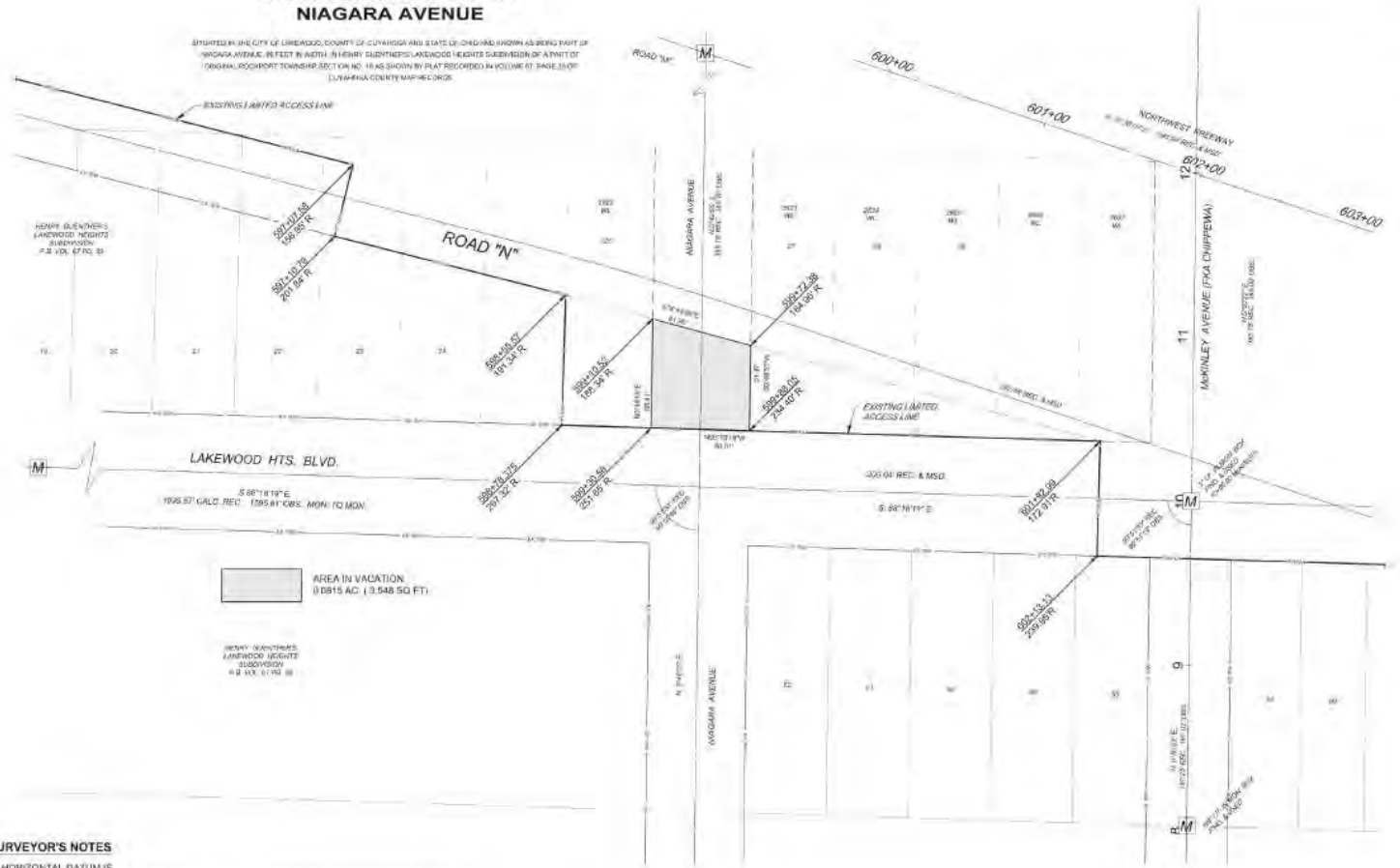
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This legislation is an important step in facilitating ODOT’s transfer of the parcels to the City. Thank you.

Respectfully,  
*Ernest E. Vargo*  
Ernest E. Vargo

## VACATION OF A PART OF NIAGARA AVENUE

SHOWN BY THE CITY OF LAKESIDE, COUNTY OF CUYAHOGA AND STATE OF OHIO HAS BEEN AS BEING PART OF  
NIAGARA AVENUE, 86 FEET IN WIDTH, IN HERBY SUBDIVISIONS LAKEWOOD HEIGHTS SUBDIVISION OF A PART OF  
ORIGINAL BUCHHEIT TOWNSHIP SECTION NO. 18 AS SHOWN BY PLAT RECORDED IN VOLUME 67, PAGE 55 OF  
CUYAHOGA COUNTY MAP RECORDS.



**AREA IN VACATION**  
0.0815 AC (3,548 SQ FT)

**SURVEYOR'S NOTES**

- 1.) HORIZONTAL DATUM IS
- 2.) PERTINENT RECORDS USED ARE AS SHOWN HEREON AND INCLUDE CUYAHOGA COUNTY TAX MAP.
- 3.) THIS MAP AND THE SURVEY ON WHICH IT IS BASED HAVE BEEN PREPARED WITHOUT THE BENEFIT OF A TITLE REPORT AND ARE NOT INTENDED TO REFLECT ALL EASEMENTS, ENCUMBRANCES, OR OTHER CIRCUMSTANCES AFFECTING THE TITLE TO THE PROPERTY SHOWN HEREON.

**SURVEYOR'S CERTIFICATION**

I HEREBY CERTIFY THAT THE SURVEY SHOWN HEREON IS THE RESULT OF AN ON-THE-GROUND SURVEY THAT HAS BEEN PERFORMED BY ME OR UNDER MY SUPERVISION THAT THE SURVEY WAS PREPARED IN ACCORDANCE WITH CHAPTER 4733-07 (MINIMUM STANDARDS FOR BOUNDARY SURVEYS) OF THE OHIO ADMINISTRATIVE CODE.

THOMAS M. MEERS  
PROFESSIONAL SURVEYOR OHIO REG. NO. 88674  
DATE OF SURVEY:

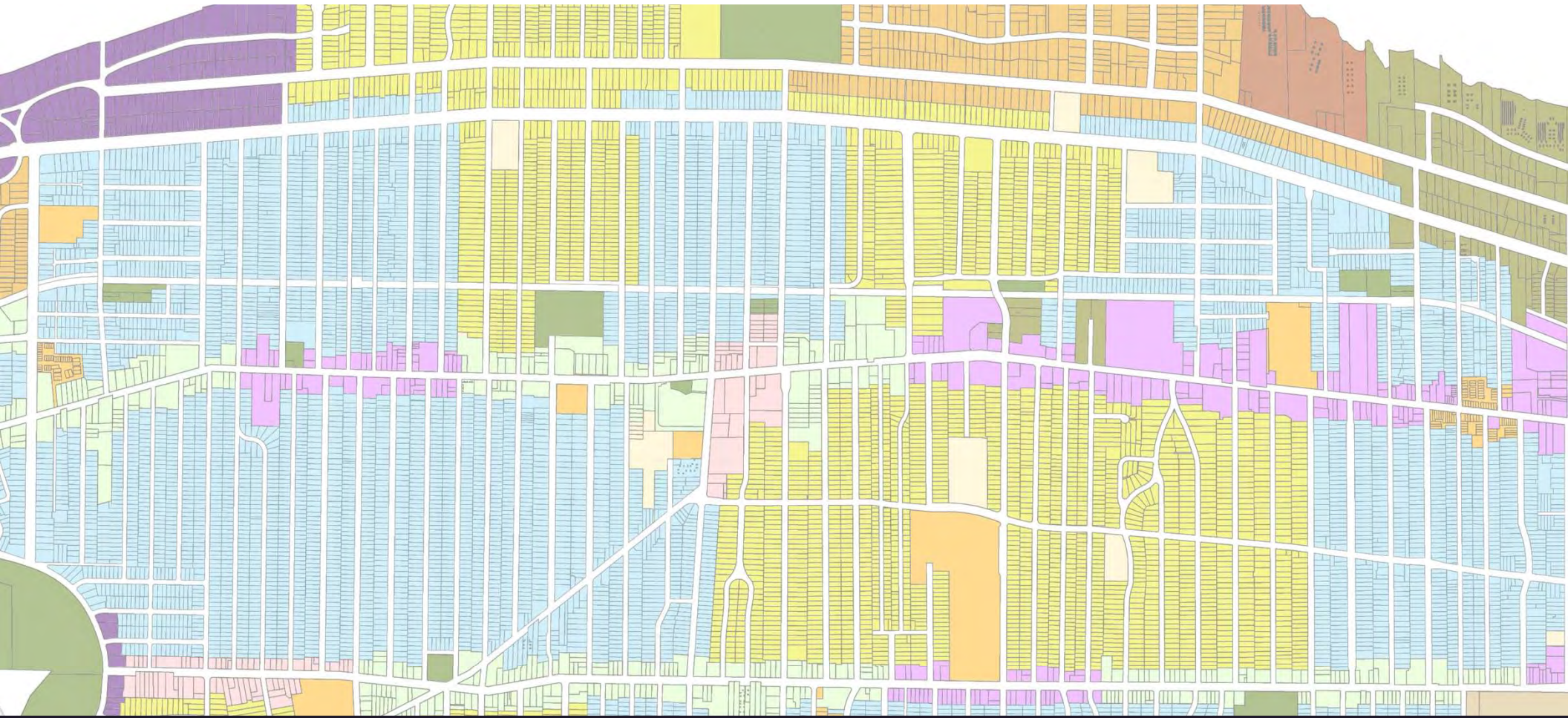


THIS SURVEY PLAT COMPLES WITH THE CUYAHOGA COUNTY  
TRANSFER AND CONVEYANCE STANDARDS AND IS HEREBY APPROVED  
PLAT VOL. \_\_\_\_\_ PG. \_\_\_\_\_  
T.M. \_\_\_\_\_ AGENT

**PORTION OF NIAGARA AVENUE  
TO BE VACATED**  
CITY OF LAKEWOOD  
COUNTY OF CUYAHOGA  
STATE OF OHIO

PROJECT NO.	241325
DATE	05/04/2024
TYPE OF SURVEY	CIVIL
DATE OF SURVEY	05/04/2024
SHEET NO.	VACATION
SHEET TOTAL	1





**Planning Commission**  
November 2024

